

# **APPLICATION ACCEPTANCE POLICY**

## **CONDITIONAL ZONING MAP AMENDMENT**

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**City of Wilmington  
Planning and Development**

**PO Box 1810 | 305 Chestnut St. Wilmington, NC 28402  
Telephone 910.254.0900 | Fax 910.341.3264**

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The policies outlined below enable planning staff to move an application along in a way that ensures that each application receives the attention it deserves. Staff desires to complete review of projects in an accurate and timely manner. Due to the volume of applications and public hearing schedules, working with incomplete materials detracts from the timely review of applications.

1. Applications will be reviewed for completeness by staff prior to being officially accepted for review.
2. Checklists for each type of request are provided with each application package. If the application does not include all required items on the checklist, it will be considered incomplete and shall not be accepted.
3. Upon determination by staff that an application is complete, it will be officially accepted by the department. Staff will complete an acceptance form and both staff and the applicant must sign the form. The application is not considered officially accepted until this form is signed by both the planner and the applicant. A copy of the signed form will be given to the applicant and a copy will be placed in the project file at the time of acceptance. Staff will not hold materials for incomplete applications.
4. Application fees must be paid at the time an application is submitted for acceptance.
5. To allow time to process fees, applications will not be accepted after 4:00 PM each day. On the deadline day for submittals, applications will not be accepted after 1:00 PM.
6. All applications require a pre-application meeting. To schedule an appointment with staff, please call 910-254-0900. Please allow sufficient time to review the application package with staff.
7. A flowchart for zoning map amendments is included on the last page.

Staff looks forward to working with you during the application process.  
If you have questions or need further assistance, please call 254-0900.

**APPLICATION FOR  
CONDITIONAL  
ZONING MAP AMENDMENT**



City of Wilmington  
Planning and Development

P. O. Box 1810, 305 Chestnut St,  
Wilmington, NC 28402  
Telephone: (910) 254-0900 FAX: (910)341-3264

**Section A. APPLICANT INFORMATION**

NAME OF APPLICANT: HHHunt Corporation (contact - Tyler J. Womble)

MAILING ADDRESS OF APPLICANT: 11237 Nuckols Rd, Glen Allen, Virginia 23059

PHONE NUMBER AND EMAIL OF APPLICANT: (804) 762-4800 ext.81270 tjwomble@hhhunt.com

**PROPERTY OWNER INFORMATION:**

Name(s)	<u>Paula Honeycutt</u>	<u>Turtle Trail Holdings, LLC</u>	<u>Eduardo Zavala and wife,</u>
	<u>206 Headwaters Dr</u>	<u>4026 Chapra Dr</u>	<u>7729 Alexander Rd</u>
Address:	<u>Hampstead, NC 28443-2090</u>	<u>Wilmington, NC 28412</u>	<u>Wilmington, NC 28411</u>
			<u>Zip</u>

Telephone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email address: \_\_\_\_\_

**PROPERTY INFORMATION:** The following information is required to provide the necessary information to process the rezoning request:

ADDRESS OF SUBJECT SITE: 7725, 7729, 7753, 7759 Alexander Rd and an adjacent parcel

NEW HANOVER CO. PROPERTY IDENTIFICATION # (PIN): R03600-003-009-002; R03600-003-117-000; R03600-003-008-001; R03600-003-008-000; R03600-003-009-001

CURRENT ZONING DISTRICT(S): R-15 PROPOSED ZONING DISTRICT(S): MD-17

TOTAL SITE AREA: +/-19.4 acres

PROPOSED USE(S) AND PROPOSED CONDITIONS FOR THE SITE:  
PRE-APPLICATION MEETING: 2/23/2024 253 residential units  
(Date)

NEIGHBORHOOD MEETING: 3/12/2024 (report due prior to application submittal)  
(Date)

INTERNAL USE ONLY	
DATE RECEIVED: _____	PLANNER: _____
CASE FILE #: _____	FEE PAID \$: _____

It is important that the applicant provide information to explain how the rezoning request satisfies the following questions. Please attach additional sheets if necessary.

1. **What changed or changing condition(s) justifies the passage of the amendment?** Discuss how circumstances have so changed since the property was last zoned.

See attached.

2. **Explain how the map amendment would be consistent with the *Create Wilmington Comprehensive Plan* and adopted special area plans (e.g., corridor plans, neighborhood plans, etc.).**

See attached.

3. **Briefly explain the expected impacts on the areas if the proposed zoning map amendment is approved.**

See attached.

4. **Describe any other circumstances that justify that the proposed amendment in the public interest.**

See attached.

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## **Section B. SUBMITTAL INFORMATION AND PROCEDURE**

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- (1) Supplementary information is **REQUIRED** as part of the application. See Section C below for required information.
- (2) All applications require a pre-application meeting prior to submittal. Applications may be submitted after pre-application meetings and all applications **MUST** be reviewed by planning staff for completeness **PRIOR** to the acceptance. Please **do not** leave your application materials without speaking to a planner. Applications left this way will not be processed or scheduled for planning commission review.
- (3) A receipt for applicalbe fees must accompany the application.
- (4) Applications shall be submitted according the the adopted filing calendar and will be considered by the planning commission at the regular meeting noted on the official acceptance form.
- (5) The applicant or agent should appear at all hearings. Planning commission meetings are typcially held at 6:00 P. M. in at City Hall, council chambers (second floor) 102 N. 3<sup>rd</sup> Street, Wilmington, North Carolina, usually on the first Wednesday of each month. Applicants will be informed of any change in date, time, or location of meetings.
- (6) A written summary of at least one neighborhood meeting (Section 18-567), for which notification was sent to property owners within 300 feet of the subject site, must be submitted to the city clerk before an application will be accepted.
- (7) Conceptual site plan review (pre-TRC consultation) by the technical review committee (TRC) is required for all conditional district map amendment applications. To avoid potential processing and scheduling delays, applicants are advised to have completed a conceptual review with the TRC prior to rezoning application submittal. If a conceptual review has not occurred at time of application, the proposed development will be scheduled for conceptual review at the next available TRC meeting.

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## **Section C. REQUIRED SUPPLEMENTAL INFORMATION**

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**PLEASE INCLUDE ALL OF THE FOLLOWING INFORMATION.** Please check the list below carefully before you submit:

- Agent form if the applicant is not the property owner;
- Detailed project narrative describing the site, proposed rezoning, uses, and any applicable prescribed conditions;
- One digital copy (pdf) of the site plan, drawn to scale, that includes, at minimum, the site and proposed building information as indicated in Section D below;
- Legal description of property requested for rezoning, by metes and bounds;
- Receipt of application fee;
- List of the names of owners, their addresses, and the tax parcel numbers of the properties within 300 feet of the subject property, including those separated by a street right-of-way. A list will be provided by planning staff;
- Two sets of business-size envelopes pre-addressed to the property owners within 300 feet of the subject property with the planning department's return address. All envelopes must have postage. **Metered postage must be updated.** All envelopes should include the department's return address:  
     City of Wilmington Planning Department  
     PO Box 1810  
     Wilmington, NC 28402-1810
- Current to-scale copy of the New Hanover County tax map delineating the subject property.

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## **Section D. INFORMATION REQUIRED TO BE INCLUDED ON THE SITE PLAN**

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The following information is required to be shown on the site plan submitted with this application. If any of the required information is not included on the site plan, reasons for excluding that information must be given.

- A boundary survey and vicinity map showing the property's total acreage, its zoning classification(s), the general location in relation to major streets, railroads, and waterways, the date and north arrow;
- All existing easements, reservations, and rights-of-way, existing and proposed;
- Approximate location on the site of proposed buildings, structures, and other improvements;
- Approximate dimensions, including height, of proposed buildings and structures;
- Proposed use of land and buildings, including the number of residential units and the total square footage of any nonresidential development;
- All required and proposed yards, buffers, screening, and landscaping;
- All existing and proposed points of access to public streets;
- Delineation of areas within the regulatory floodplain as shown on official flood hazard boundary maps;
- Proposed phasing, if any;
- The location of existing and proposed storm drainage patterns and facilities intended to serve the proposed development;
- Approximate location of all existing and proposed infrastructure on the site including water, sewer, roads, and bicycle and pedestrian facilities;
- Generalized traffic, parking, and circulation plans;
- Tree survey; and
- Traffic impact analysis, if required.

**In evaluating the proposal, staff, planning commission, and city council may request additional information from the applicant. Additional information may include the following:**

- Proposed screening, buffers, and landscaping over and above that required by the land development code, as well as proposed treatment of any existing natural features;
- Scale of buildings relative to abutting property;
- Building elevations and exterior features of proposed development;
- Any other information needed to demonstrate compliance with the land development code; and
- Proposed number and locations of signs.

**OWNER'S SIGNATURE\*:** *In filing this application for a zoning map amendment, I/we as the property owner(s), hereby certify that all information presented in this application is accurate to the best of my knowledge, information, and belief. I hereby designate Tyler J. Womble to act on my behalf regarding this application, to receive and respond to administrative comments, to resubmit plans on my behalf, and to speak for me in any public meeting regarding this application.*

Signature/Date:   
 PAULA HONEYCUTT

TURTLE TRAIL HOLDINGS LLC, a North Carolina  
 limited liability company

Signature/Date: \_\_\_\_\_  
 Name: \_\_\_\_\_  
 Title: \_\_\_\_\_

Signature/Date: \_\_\_\_\_  
 EDUARDO ZAVALA

Signature/Date: \_\_\_\_\_  
 Name: \_\_\_\_\_

In evaluating the proposal, staff, planning commission, and city council may request additional information from the applicant. Additional information may include the following:

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Signature/Date: \_\_\_\_\_  
 PAULA HONEYCUTT

TURTLE TRAIL HOLDINGS LLC, a North Carolina  
 limited liability company

Signature/Date: \_\_\_\_\_  
 Name: \_\_\_\_\_ RICHARD J. MATS  
 Title: \_\_\_\_\_ MEMBER

Signature/Date: \_\_\_\_\_  
 EDUARDO ZAVALA

Signature/Date: \_\_\_\_\_  
 Name: \_\_\_\_\_

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Signature/Date: \_\_\_\_\_  
 PAULA HONEYCUTT  
  
 TURTLE TRAIL HOLDINGS LLC, a North Carolina  
 limited liability company

Signature/Date: \_\_\_\_\_  
 Name: \_\_\_\_\_  
 Title: \_\_\_\_\_

Signature/Date:  3/12/24  
 EDUARDO ZAVALA

Signature/Date:  03/12/24  
 Name: Karina Davila De Suarez.





**NEIGHBORHOOD MEETING FOR CONDITIONAL DISTRICT REZONING PROPOSAL**

To: Penelope Spicer-Sidbury, City Clerk

Case No.: \_\_\_\_\_

Location: Ogden Baptist Church, 1721 Market Street

Proposed zoning: MD-17

I hereby certify that written notice of a neighborhood meeting on the above conditional district map amendment application was mailed to property owners with 300 feet, as set forth on the attached list, by first class mail on (date) March 1, 2024. A copy of the written notice is also attached.

The meeting was held at the following date, time and location:  
March 12, 2024 from 6:00 to 7:00pm at Ogden Baptist Church, 1721 Market Street

The persons in attendance at the meeting were: Please see attached sign-in sheets.  
\_\_\_\_\_  
\_\_\_\_\_

The following issues were discussed at the meeting: Please see attached.  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

As a result of the meeting, the following changes were made to the rezoning application: \_\_\_\_\_  
Please see attached.  
\_\_\_\_\_  
\_\_\_\_\_

Date: 3/15/2024  
Applicant: HHHunt Corporation  
By: Tyler J Womble DocuSigned by: Tyler J. Womble

Please submit this report to the Wilmington City Clerk's office located at 102 North 3<sup>rd</sup> Street or mail it to Penelope Spicer-Sidbury, City Clerk, PO Box 1810, Wilmington, NC 28402-1810.



## Neighborhood Meeting for Conditional Rezoning Request

6:00-7:00 p.m.  
March 12, 2024:

Ogden Baptist Church  
Fellowship Hall  
7121 Market Street  
Wilmington, NC 28411

We invite you to join us for a neighborhood meeting for the proposed conditional rezoning of approximately 19.4 acres located at 7731 Alexander Road, Wilmington, North Carolina and identified as New Hanover County Parcels: R03600-003-008-000; R03600-003-009-001; R03600-003-009-002; R03600-003-117-000; R03600-003-008-001. The current zoning of the property is R-15. The proposed conditional rezoning is for MD-17, multi-family for approximately 253 residential units.



We welcome the opportunity to discuss the proposed project with you at the neighborhood meeting. This will be an open-house style meeting with members of the development team available at various stations to discuss the project and answer any questions. We look forward to seeing you.

**Because it's how you live that matters**













**Community Meeting Summary**  
**for Conditional Rezoning Request by HHHunt Corporation**  
Meeting Date & Time: Tuesday, March 12, 2024, 6:00 PM -7:00 PM  
Meeting Location: Ogden Baptist Church, 1721 Market Street, Wilmington, NC

HHHunt Corporation, McKim & Creed and DRMP (collectively, Development Team) hosted a meeting to introduce the proposed development for Abberly Landing located at 7731 Alexander Road, Wilmington, NC to the adjacent neighbors attending the meeting.

The community asked the Development Team questions about the plans, and many of the same questions were discussed repeatedly during the meeting. The following is a synopsis of the comments heard at the meeting.

- Attendees questioned what the building heights would be. The Development Team explained that the garages along the property edge are 1 story, the outer apartment buildings are 3 story that are set back from the property boundary with parking areas and the U-shaped center apartment building is 4 story.
- Some attendees were concerned about the screening between the apartments and the neighboring homes on Alexander Place, and asked whether the buffer fence could be increased from 6' to 8' height.
- Concerns were raised over the impact traffic will have on Alexander Road, and the benefit of adding U-Turns on Market Street. The Development Team explained the U-Turns will reduce conflict intersections and make for safer travel along Market Street.
- Another concern regarding traffic was why there was a road connection to Alexander Place. The Development Team explained that the access road was for emergency access only and will be gated, and access limited to service and emergency vehicles.
- Another question was asked regarding speed bumps to help control speed. The Development Team explained that NCDOT has strict regulations on speed bumps and none are proposed with the rezoning.
- There were questions regarding how the stormwater will be handled on the site and whether neighboring properties will be able to connect to the sewer. The Development Team explained the direction of the flow of water, the function of the onsite pond and the overflow to the creek. The Development Team also stated the neighboring properties will not have sewer connection but advised them they can pursue sewer connection for their properties on their own.
- Concerns were raised regarding flooding of two nearby vacant lots on Alexander Place. The Development Team explained stormwater management requirements and is looking into the areas mentioned.
- An attendee asked if the property has been purchased yet and if the apartment complex would result in decreased property value. The Development Team stated it has not been purchased yet but is under contract and that the value of the property would be vastly increased with apartments over what currently exists on the property.
- An attendee asked if there is anything that can be done to reduce construction noise. The Development Team stated there would be a construction representative on site if there were issues with noise at the time, and also let them know there are noise ordinances in place.
- There was an attendee that asked if notifications will be mailed to them letting them know when the planning board meeting will take place. The Development Team explained the notices will be mailed to them and the rezoning process.



**Community Meeting Summary**  
**for Conditional Rezoning Request by HHHunt Corporation**  
Meeting Date & Time: Tuesday, March 12, 2024, 6:00 PM -7:00 PM  
Meeting Location: Ogden Baptist Church, 1721 Market Street, Wilmington, NC

- An attendee asked where the nearest hydrant was located. The Development Team pointed out the location of hydrants on the site plan.
- Several attendees asked if their properties were going to be annexed into the City of Wilmington with this request. The Development Team stated that their properties were not subject to the annexation and could only be annexed if they applied for it themselves as the owner.

As a result of the meeting, the following changes were made to the rezoning application:  
The height of the fence has been increased from 6 feet to 8 feet on the side of the property that faces the adjacent property owners.

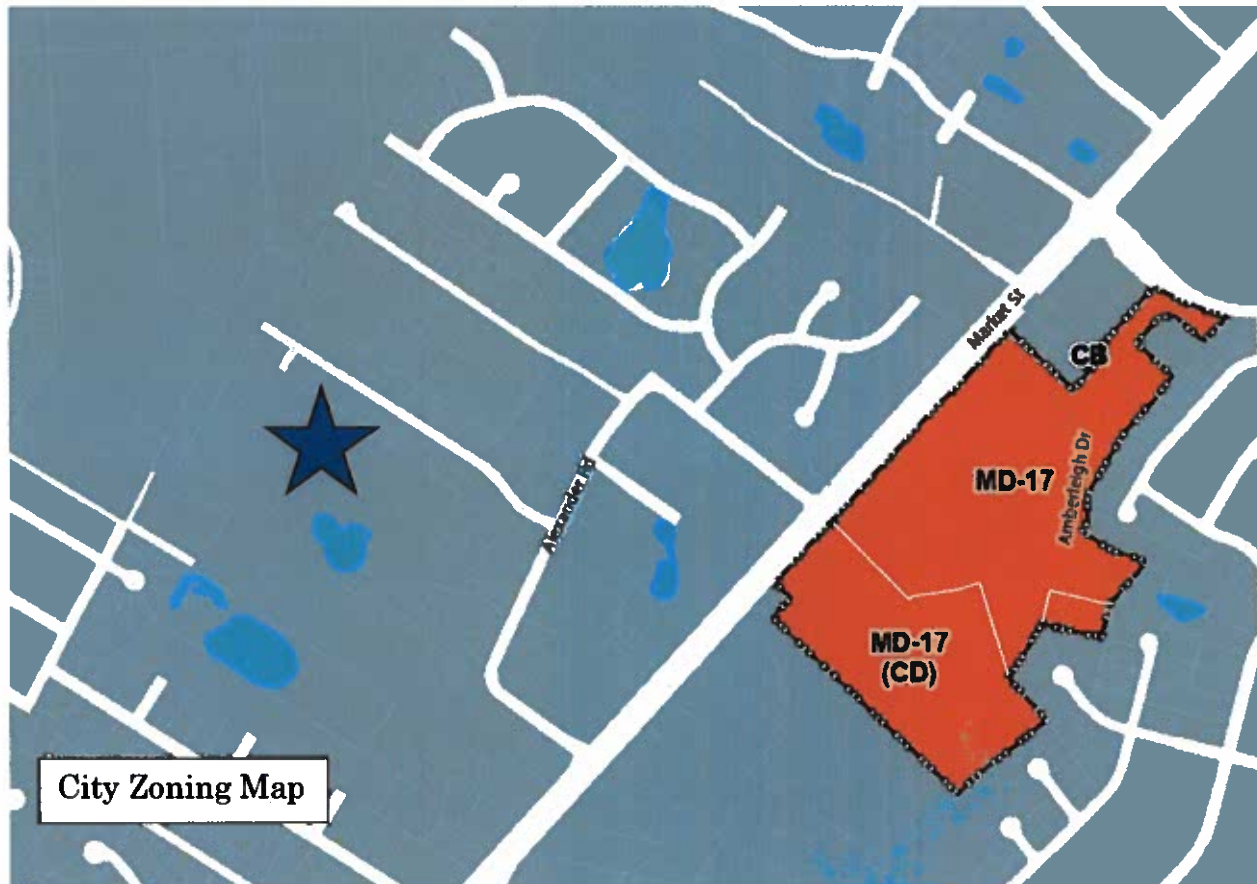




**NEIGHBORHOOD MEETING  
FOR CONDITIONAL REZONING**  
FOR PROPERTY AT 7751 ALEXANDER RD  
AND W/100 200th ST  
ON PARCELS 120, 1201 00  
SCHEDULED FOR THURSDAY, FEBRUARY 2008  
7:00 PM AT 7751 ALEXANDER RD

**1. What changed or changing condition(s) justifies the passage of the amendment? Discuss how circumstances have so changed since the property was last zoned.**

Since the property was last zoned R-15, water and sewer are now available to the area. In addition, this proposed rezoning request is across Market Street from another non-contiguous annexation property which is also zoned MD-17. The North Carolina Department of Transportation (NCDOT) is also currently constructing improvements to Market Street and surrounding rights of way which will improve the traffic conditions of the area.



The 2022 *City of Wilmington & New Hanover County, North Carolina Housing Needs Assessment Update*, by Bowen National Research shows a significant need for a range of housing types at various price points in all areas of the city and county. The residents of the City of Wilmington have been facing extreme increase in the cost of living over the last several years. This has led to a scarcity in affordable rentals within the city limits and/or closely surrounding the city limits. As such, the recent rapid inflation has intensified the importance of developing affordable and workforce housing for Wilmington citizens. Therefore, as part of this proposal and as a



testament to HHHunt’s commitment to the overall community, this project will include 10% of the residential units at 80% AMI for a period of 15 years.

**Bowen Report**

**Table 37**

**City of Wilmington, North Carolina  
Rental Housing Gap Estimates by Affordability Level  
2022 to 2032**

Percent of AMH	30%	31%-50%	51%-60%	61%-80%	81%-120%	121%+
Household Income	< \$25,560	\$25,561- \$42,600	\$42,601- \$51,120	\$51,121- \$68,160	\$68,161- \$102,240	\$102,241+
Rent Range	<\$639	\$640-\$1,065	\$1,066-\$1,278	\$1,279-\$1,704	\$1,705-\$2,556	\$2,557+
Household Growth	<b>-2,980</b>	<b>-280</b>	<b>-116</b>	<b>1,088</b>	<b>2,272</b>	<b>2,942</b>
Balanced Market	<b>1,030</b>	<b>636</b>	<b>292</b>	<b>38</b>	<b>50</b>	<b>36</b>
Replacement Units	<b>1,978</b>	<b>610</b>	<b>140</b>	<b>78</b>	<b>50</b>	<b>18</b>
External Support	<b>1,050</b>	<b>646</b>	<b>298</b>	<b>334</b>	<b>418</b>	<b>318</b>
Step-Down Support	<b>806</b>	<b>-500</b>	<b>462</b>	<b>626</b>	<b>262</b>	<b>-1,656</b>
Pipeline Units	<b>0</b>	<b>0</b>	<b>-416</b>	<b>-1,134</b>	<b>-1,558</b>	<b>-1,333</b>
Current (2022) Housing Gap (Units)	<b>1,884</b>	<b>1,112</b>	<b>660</b>	<b>1,030</b>	<b>1,494</b>	<b>325</b>
Original (2020) Housing Gap (Units)	<b>1,603</b>	<b>1,383</b>	<b>778</b>	<b>399</b>	<b>618</b>	<b>566</b>

**2. Explain how the map amendment would be consistent with the Create Wilmington Comprehensive Plan and adopted special area plans (e.g., corridor plans, neighborhood plans, etc.).**

The proposed conditional rezoning request from R-15 to MD-17 for a density of 13 units per acre complies with multiple plans and policies in the Create Wilmington Comprehensive Plan and other adopted plans. It would help to revitalize commercial corridors and blighted areas through infill development while promoting environmentally friendly growth by clustering development and minimizing impacts on natural resources and it would provide housing opportunities close to transit, employment, and services.

The community is planned with tree preservation areas as buffers and to gradually decrease building heights from the center of the property to the edge of the property to minimize impacts on the adjacent property owners.

**Reduce Infill Development Impacts with Buffers and Transitions**

The relationship of sites to one another is a critical factor in city building. The impacts of infill development and increases in density can be minimized with appropriate site design techniques. This is not limited to distance alone or thick vegetation areas, which often prevent neighborhood cohesion and walkability. Excessive distances between residential and commercial land uses have unintended consequences, such as traffic congestion.

Numerous solutions are available to achieve walkable, mixed-use centers and corridors without negatively impacting adjacent lower density areas, these are just a few:

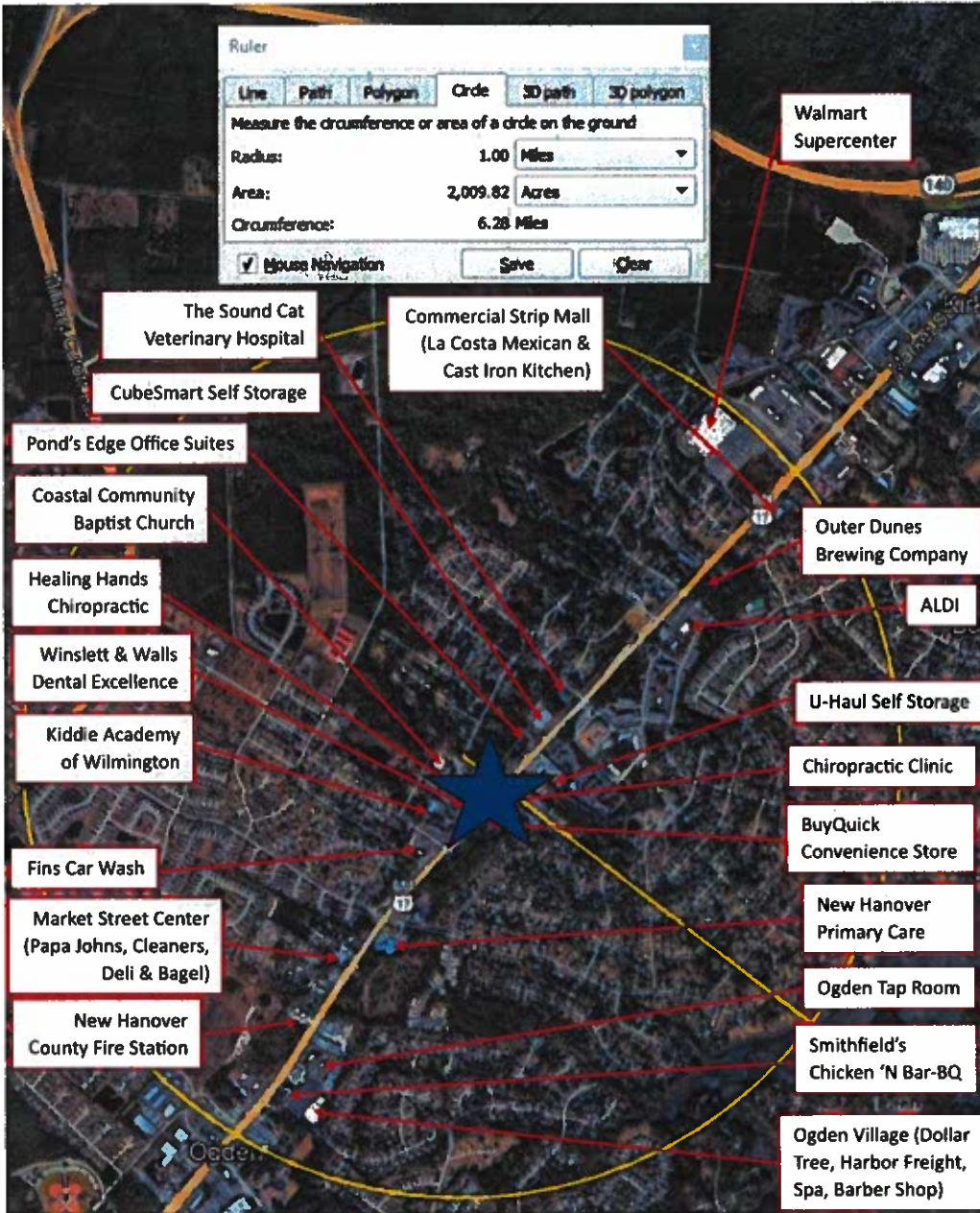
**B Greenways (Off-Street Multi-use Pathways)**

Land dedicated to greenway corridors can be used as active, linear open space between transitional densities.

**C Vegetated Buffer**

The use of heavy plantings can "screen" one site from another. Pedestrian connections can be used in some locations.





The proposed conditional rezoning property is located along a High-Capacity Transit Route and a Greenway Connection which calls for higher density residential uses. The MD-17 zoning district, with a density of 13 units per acre would also provide a transition between the commercial uses along Market Street and the lower density residential uses. The Wilmington/New Hanover County Comprehensive Greenway Plan specifically calls out for interconnectivity for the Market Street Rail Trail and recommends providing alternate networks of pedestrian and cyclist amenities through neighborhoods. The proposed rezoning project includes a bicycle maintenance building and adds a sidewalk connection from within the community, along Alexander Road, and to the Market Street Rail Trail which allows residents of



the neighborhood better access to the surrounding goods and services as well as Ogden Park without adding a driveway intersection to the trail. This trail is currently under construction as part of NCDOT's Market Street improvements.

**#13: MARKET ST RAIL TRAIL**

This 3.8-mile trail connects Ogden Park and the Military Cutoff Road Trail with northeast neighborhoods.

**WHY IT'S IMPORTANT:**

- Direct Access to/from
  1. An Existing or Funded Trail
  2. An Elem., Middle, High School or Library
  3. Major Transit Routes
  4. Higher Density Residential Areas
  5. Higher Density Employment Areas or Major Employer Centers
  6. A Park or Recreation Center
  7. Major Shopping Area
- Serves areas outside of the existing trail service areas
- Corridor contains High Number of Bikes/Ped Accidents
- Relates to the top 10 recommended trail locations from the Public Comment Form
- Relates to the top 10 recommended trail locations from Stakeholder Interviews



**WHAT WILL IT COST?**

A corridor-specific study is needed for accurate costs. Planning-level costs at \$600K/mile come to **\$2.3M**. Additional costs may apply due to the constraints noted at right.

**COMPREHENSIVE GREENWAY PLAN**



Trail would connect to the Parter's Neck neighborhood and shopping areas to the north.

Driveways that intersect the trail should be clearly marked for both motorists and trail users, especially in commercial areas.

Trail would follow along the NW side of Market St., in the former rail ROW wherever possible (ROW as seen from Alexander Rd below).

Trail would connect directly into Ogden Park

Trail would connect to the Middle Sound neighborhoods, the Military Cutoff Rd trail and shopping areas to the south with a trail overpass across Market St.

Based on the community enhancements from the proposed project as shown in the application and associated documents, the following policies, among others within the comprehensive plan, offer strong support for the proposed conditional zoning request:



**1.2.1: Development should create places, streets, and spaces that are visually attractive, safe, accessible, functional, inclusive, have their own distinct identities, and maintain or improve desired character.**

**1.2.2: Development should occur in a compact pattern that reinforces the efficient provision of public services and utilities, improves the performance of the transportation network, preserves open space, and reduces negative impacts of low-intensity and non-contiguous development patterns.**

**1.2.3: Large-Scale development should include provisions for parks, community facilities, and other amenities to help meet the demands created by the development.**

**1.2.4: Development and infrastructure investments should promote healthy communities and active lifestyles by providing enhanced bicycle and pedestrian circulation, access, and safety.**

**1.2.6: Cluster development should be encouraged, where appropriate, to achieve open space preservation or creation, especially in those parts of the city with special environmental concerns, less urban development patterns, or flooding concerns.**

**1.4.1: Quality design and site planning should be promoted so that new development infill and redevelopment is implemented with minimal adverse impacts on desired character of the existing built environment.**

**1.4.4: Low- to medium-density residential areas and/or low-intensity mixed-use developments should serve as transitions between lower-density neighborhoods and more intensive commercial, residential, and mixed-use areas. Where appropriate transitions are not possible within a development site, infill and redevelopment adjacent to areas of lower intensities should provide effective physical buffers to avoid adverse impacts.**

**1.7.1: Growth should be accommodated in the city through mixed-use neighborhoods with a variety of housing types and price points.**

**1.7.2: Revitalization and stabilization of neighborhoods at risk should be facilitated through grants, loans, housing rehabilitation efforts, commercial investment programs, capital improvements, public-private partnerships, and other actions.**

**1.7.8: Usable open space that preserves natural landscapes and high-quality ecological resources should be encouraged in new residential development.**

**1.7.9: Infill development should enhance or improve the existing physical development pattern of the area. The development of lots that have been historically difficult to infill or redevelop should be facilitated.**

**1.7.10: Infill sites within existing neighborhoods should be developed consistently with the design elements of nearby structures and in a manner that enhances or improves the character of the area.**

**1.11.3: Areas well-suited for infill and redevelopment, should be redeveloped in a way that maintains or enhances the desired character of the surrounding area, improves access to goods, services, and amenities, increases housing options, and improves the overall quality of life in the vicinity.**

**2.6.1: Bicycle and pedestrian circulation, access, and safety should be enhanced, especially along corridors, downtown, in activity and employment centers, within densely-developed areas, at transit stations, and near schools, libraries, and parks.**

**2.6.2: A continuous bicycle and pedestrian network should be provided within and between existing and new developments to facilitate safe and convenient travel. New subdivisions, mixed-use developments, and large-scale commercial developments should include safe pedestrian walkways or multiuse paths that allow direct links between roadways and major destinations, transit stops, and schools.**

**2.6.3: New development, redevelopment, street reconstruction, and resurfacing projects should include bicycle and pedestrian facilities as appropriate for the roadway character. Existing development should be retrofitted with connections where possible.**

**2.7.6: Parking lots should include vehicular and pedestrian connections between and through lots. Parking facility quality should be considered equally with quantity of parking spaces. Parking lot design should minimize pedestrian conflicts, make use of appropriate landscaping, and properly manage stormwater.**

**3.1.1: Mixed-income neighborhoods should be promoted throughout the city, particularly within identified high-density centers and corridors, downtown, and near employment centers, and by dispersing, rather than concentrating low to moderate income housing.**

**3.1.2: Dispersal and production of affordable and workforce housing units, housing with universal design elements, and senior housing units should be promoted throughout all areas of the city.**

**3.1.3: Quality design and appearance for all housing, including assisted and market rate housing, should be encouraged.**

**3.1.4: Zoning policies should provide opportunity and incentives for developers to build a variety of housing types, from single-family to multifamily, including accessory dwelling units, at a range of price points.**

**4.2.2: Neighborhood connectivity to trails and greenways should be facilitated. Connections between neighborhoods, shopping centers, schools, transit stops, and employment centers should function as transportation alternatives in addition to recreational amenities.**

**4.2.5: Wilmington's greenway/trail network should include multi-use paths that connect other greenways, parks, and schools. New greenways should be designed to serve both recreational and transportation needs.**

**5.2.1: In partnership with neighborhood groups, focused reinvestments to make safe, attractive, and walkable neighborhoods and attract skilled workers to Wilmington should be encouraged. A mix of housing types and price points should be encouraged to diversify neighborhoods, particularly around business clusters and schools.**

**6.2.2: Opportunities and incentives to mitigate the loss of sensitive areas that occur as part of the development process should be identified. Impact reduction and avoidance measures should be based on criteria that take environmentally sensitive natural areas and habitats into consideration.**

**6.2.4: The design and construction of public facilities such as roads, sewers, or stormwater control, should utilize best management practices to minimize impacts on sensitive natural areas, open space, and conservation areas.**

**6.3.1: Tree preservation should be a priority in developments where significant clusters of trees can be protected for meaningful impact and appearance. Planting of street trees with frequent spacing should be a priority where the density of buildings, infrastructure, and required grading make the preservation of existing trees unfeasible.**

**6.4.1: Walkable and bikeable communities, public transit, and integrated land use and transportation planning should be promoted and encouraged to help reduce motor vehicle emissions.**

**8.1.17: Growth and redevelopment should be encouraged in areas already served by adequate utility infrastructure. Utilities extension policies that maximize environmental sensitivity, encourage greater infill and redevelopment in already urbanized areas, and protect rural and undeveloped areas should be utilized.**

9.2.1: The strategic transformation of public and private spaces into meaningful places through placemaking should be encouraged. Wilmington should have a robust, well-connected network of public and semi-public spaces.

9.4.3: Parking lot landscaping and planting should be utilized to provide effective shade canopy, to help screen the view of parking, improve stormwater infiltration and reduce stormwater runoff, and reduce the heat island effect.

9.5.1: As population growth leads to new housing and development patterns, new development within established neighborhoods should address the existing distinctive character of the surrounding area, while allowing transitional changes.

9.5.2: Clear and safe pedestrian networks within, through, and between neighborhoods should be enhanced. Opportunities to connect existing neighborhoods to adjacent commercial centers and community facilities and services should be explored.

9.5.4: Infill and redevelopment should respect and improve the integrity of neighborhood open spaces and public areas. Buildings should be designed to avoid the loss of sunlight or vistas and enhance the usability of neighborhood parks, plazas, and buffers.

9.5.5: Buffers and gradual building height changes should be used to transition between established neighborhoods and higher-density development, though this should not disrupt or prevent connections between compatible and complementary land uses.

9.6.1: New housing of mixed types and prices should be encouraged in proximity to and within employment areas, Mixed-use Centers, and Neighborhood Nodes, as identified on the Growth Strategies Maps.

9.6.3: Infill and redevelopment should enhance the desirable characteristics that make each part of Wilmington unique. New neighborhood centers and mixed-use developments should use one or more “main streets” and public gathering spaces as a central focal point.

It should be noted a variety of amenities and services are within walking and biking distance of the proposed project to create a compact pattern that reinforces the efficient provision of public services and utilities while preserving open space and providing community facilities and other amenities to help meet the demands created by the development. As such, this project will revitalize commercial corridors through infill development that is compatible with surrounding neighborhoods and creating density that can support future growth.

**3. Briefly explain the expected impacts on the areas if the proposed zoning map amendment is approved.**

The proposed rezoning would have a beneficial impact on the area if approved. The community will provide a much-needed mix of housing types as contemplated by the Comprehensive Plan. The proposed change would also allow for a more modern development pattern of the area as the development would have to comply with all of the City of Wilmington's land use and planning regulations including the traffic impact analysis mitigation as well as updated stormwater systems and landscaping. The community is planned with tree preservation areas as buffers and to gradually increase in height so as to have minimal impact on the adjacent property owners.

The property is in close proximity to a greenway trail as well as multiple employment and retail opportunities. The higher intensive land use and increase in density would be much more suitable and blend well with the surrounding area. Ultimately this project will have little or no negative impacts on the surrounding area, rather an abundance of positive impacts.

**4. Describe any other circumstances that justify that the proposed amendment in the public interest.**

Over the last several years, access to safe, decent, affordable housing has been a growing challenge nationwide, and specifically in the City of Wilmington. The local housing stock has continued to dwindle relative to the need. This project is an opportunity to bring a class-A community to the City of Wilmington which will provide safe, amenity rich housing close to goods and services.

The community will also provide multiple public benefits including constructing current stormwater management in an area which was developed prior to stormwater regulations, remediation of two localized petrochemical spills, and preservation of all on-site Specimen Trees.

**AUTHORITY FOR APPOINTMENT OF PERSON  
TO ACT ON MY BEHALF**

The undersigned owner, Paula Honeycutt, does hereby appoint Tyler J. Womble to act on my behalf for the purpose of petitioning the City of Wilmington for: a) an amendment to the text regulations; b) a change to the zoning map; c) approval of a special use permit; d) approval of a special use district; and/or, e) street closing, as applicable to the property described in the attached petition.

The owner does hereby covenant and agree with the City of Wilmington that said person has the authority to do the following acts for and on behalf of the owner:

- (1) To submit a proper petition and the required supplemental materials:
- (2) To appear at public meetings to give testimony and make commitments on behalf of the owner; and
- (3) In the case of a special use permit, to accept conditions or recommendations made for the issuance of the special use permit on the owner's property.
- (4) To act on the owner's behalf without limitations with regard to any and all things directly or indirectly connected with or arising out of any petition.

This appointment agreement shall continue in effect until final disposition of the petition submitted in conjunction with this appointment.

Date: March 18, 2014

Appointee's Name, Address & Telephone:

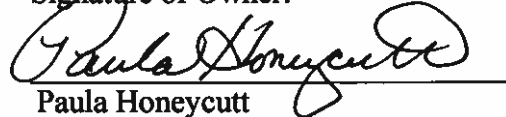
Tyler J. Womble of HHHunt

11237 Nuckols Rd

Glen Allen, VA 23059

(804) 762-4800 ext. 81270

Signature of Owner:

  
Paula Honeycutt

**AUTHORITY FOR APPOINTMENT OF PERSON  
TO ACT ON MY BEHALF**

The undersigned owner, Turtle Trail Holdings LLC, does hereby appoint Tyler J. Womble to act on my behalf for the purpose of petitioning the City of Wilmington for: a) an amendment to the text regulations; b) a change to the zoning map; c) approval of a special use permit; d) approval of a special use district; and/or, e) street closing, as applicable to the property described in the attached petition.

The owner does hereby covenant and agree with the City of Wilmington that said person has the authority to do the following acts for and on behalf of the owner:

- (1) To submit a proper petition and the required supplemental materials:
- (2) To appear at public meetings to give testimony and make commitments on behalf of the owner; and
- (3) In the case of a special use permit, to accept conditions or recommendations made for the issuance of the special use permit on the owner's property.
- (4) To act on the owner's behalf without limitations with regard to any and all things directly or indirectly connected with or arising out of any petition.

This appointment agreement shall continue in effect until final disposition of the petition submitted in conjunction with this appointment.

Date: 3/6/24

Appointee's Name, Address & Telephone:

Tyler J. Womble of HHHunt

11237 Nuckols Rd

Glen Allen, VA 23059

(804) 762-4800 ext. 81270

Signature of Owner:

Turtle Trail Holdings LLC, a North Carolina limited liability company

Name: RICHARD T. HATES

Title: MEMBER

**AUTHORITY FOR APPOINTMENT OF PERSON  
TO ACT ON MY BEHALF**

The undersigned owner, Eduardo Zavala and wife, does hereby appoint Tyler J. Womble to act on my behalf for the purpose of petitioning the City of Wilmington for: a) an amendment to the text regulations; b) a change to the zoning map; c) approval of a special use permit; d) approval of a special use district; and/or, e) street closing, as applicable to the property described in the attached petition.

The owner does hereby covenant and agree with the City of Wilmington that said person has the authority to do the following acts for and on behalf of the owner:

- (1) To submit a proper petition and the required supplemental materials:
- (2) To appear at public meetings to give testimony and make commitments on behalf of the owner; and
- (3) In the case of a special use permit, to accept conditions or recommendations made for the issuance of the special use permit on the owner's property.
- (4) To act on the owner's behalf without limitations with regard to any and all things directly or indirectly connected with or arising out of any petition.

This appointment agreement shall continue in effect until final disposition of the petition submitted in conjunction with this appointment.

Date: 03/12/2024

Appointee's Name, Address & Telephone:

Tyler J. Womble of HHHunt

11237 Nuckols Rd

Glen Allen, VA 23059

(804) 762-4800 ext. 81270

Signature of Owner:



Eduardo Zavala



Name: Karina Davila De Suarez



## **Project Narrative**

The Applicant, HHHunt Corporation (“HHHunt”) is requesting conditional rezoning of approximately 19.4 acres of land located off Alexander Road (specifically parcels: R03600-003-008-000, R03600-003-009-001, R03600-003-009-002, R03600-003-017-000, and R03600-003-008-001) from R-15 to MD-17 for a 253 multifamily residential community. The properties are currently located in the unincorporated area of New Hanover County. This proposal is part of an annexation request also submitted to the City of Wilmington.

This proposed highly amenitized, pet friendly residential community consists of six residential buildings, a clubhouse with a pool, garages, outdoor grilling stations, EV parking spaces, boat/RV parking spaces, a trash compactor, secure bicycle storage with space for bicycle repair and a dog spa.

The site is intentionally designed so that the buildings gradually increase in height from one story buildings on the perimeter of the property to a four story U-shaped residential building in the center of the community. This central building will be elevator served and include its own inviting open courtyard space for residents.

Separately, a clubhouse will provide residents with a gym quality fitness facility, indoor game room, club room with a demonstration kitchen, a secure package room with a cooler for grocery delivery and a business coworking center with breakout rooms and conference spaces.

Residents will also have access to outdoor boat storage, on site trails for pedestrian connectivity, outdoor seating, picnic areas, and a dog park; all disbursed throughout tree preservation open space areas.

The *2022 City of Wilmington & New Hanover County, North Carolina Housing Needs Assessment Update*, by Bowen National Research shows a significant need for a range of housing types at various price points in all areas of the city and county. Therefore, as part of this proposal and as a testament to HHHunt’s commitment to the overall community, this project will include 10% of the residential units at 80% AMI for a period of 15 years.

The proposed community is located within a ½ mile of many services and will provide a sidewalk connection from the community to the new greenway path along Highway 17. As part of the planning for the community, HHHunt will be working with RideMICRO on a tentative transit stop (Zone 2) at an approved location in proximity to the community entrance. The community will be served by Cape Fear Public Utility Authority and house an on-site pump station.

A Traffic Impact Analysis was submitted on March 6, 2024 to the WMPO for their review and approval. Two points of access are provided along Alexander Road with a third emergency only access to the rear of the project onto Alexander Place. All access points will be gated and perimeter fencing will be installed around the community.

HHHunt has recently formed a partnership with EnviroSpark to install EV stations throughout HHHunt's existing and future multifamily communities. This new community will provide level 2 EV charging connections for 10% of parking spaces. Through its partnership with EnviroSpark, HHHunt has the ability to add EV stations as the demand increases.

HHHunt has completed environmental studies showing current contamination from storage tanks that once held petroleum and heating oil. This proposed community will necessitate the remediation of these two localized petrochemical spills.

Stormwater management will be provided for this community through enhancements to an existing wet pond currently located on the property. This stormwater management pond will be designed to handle stormwater runoff from all proposed improvements related to this project including buildings, roads, and parking lots. Many of the existing trees throughout the community, including existing trees around the perimeter of the current stormwater management pond, will be retained, lessening rainfall runoff. Where existing trees cannot be retained, it is proposed that vegetation will be installed to improve the water quality while reducing the runoff velocity and improving downstream. The Stormwater Management System will be designed to the City of Wilmington standards to prevent downstream flooding including control of the 100-year storm within the pond.

Located on a heavily wooded assemblage, the community is designed with the natural environment in mind. All 40 Specimen Trees identified on site will remain as well as over half of the Significant Tress. In addition, the buildings will meet National Green Building Standard bronze designation. The .022 acres of delineated wetlands on site will be incorporated into the stormwater pond which will become an enjoyable water feature of the nearby green space.

**LEGAL DESCRIPTION**  
**R03600-003-009-002**

THAT CERTAIN TRACT OF LAND BEING IN THE CITY OF WILMINGTON, NEW HANOVER COUNTY, NORTH CAROLINA, SAID TRACT BEING A PORTION OF THE PROPERTY DESCRIBED IN DEED BOOK 6599 PAGE 2736 TRACT 1 OF NEW HANOVER COUNTY REGISTER OF DEEDS AND BEING MORE FULLY DESCRIBED AS FOLLOWS:

COMMENCING AT A CAPPED REBAR SET WITHIN THE RIGHT OF WAY OF ALEXANDER ROAD, SAID CAPPED REBAR HAVING THE NORTH CAROLINA STATE PLANE COORDINATES OF NORTHING: 196,997.06 AND EASTING: 2,359,772.58, IN THE CITY OF WILMINGTON, HARNETT TOWNSHIP, NEW HANOVER COUNTY, NORTH CAROLINA, THENCE N 19°14'33" E A DISTANCE OF 447.42 FEET TO A 5/8 IN IRON ROD SET ON THE WESTERLY RIGHT OF WAY LINE OF ALEXANDER ROAD, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY JULIO R. MENDOZA ET AL SHOWN IN DEED BOOK 5934 PAGE 1487 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS, SAID IRON ROD HAVING THE NORTH CAROLINA STATE PLANE COORDINATES NORTHING: 197,419.48 AND EASTING: 2,359,920.03, BEING POINT OF BEGINNING 1.

THENCE FROM POINT OF BEGINNING 1, ALONG THE RIGHT OF WAY OF ALEXANDER ROAD, S 25°16'11" W A DISTANCE OF 30.03 FEET TO THE BASE OF A LEANING 2 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY DAMARIS REBOLLO CONTRERAS SHOWN IN DEED BOOK 9801 PAGE 2516 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE N 62°05'03" W A DISTANCE OF 305.48 FEET TO A 3/4 INCH IRON ROD FOUND, SAID ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE CONTRERAS PROPERTY PREVIOUSLY DESCRIBED,

THENCE S 33°00'48" W A DISTANCE OF 17.23 FEET TO A 5/8 INCH IRON ROD SET, SAID ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE CONTRERAS PROPERTY PREVIOUSLY DESCRIBED,

THENCE S 33°00'48" W A DISTANCE OF 140.37 FEET TO A 1 AND 1/2 INCH PINCHED IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY DAMARIS CONTRERAS ET AL SHOWN IN

DEED BOOK 5968 PAGE 1386 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 32°55'17" W A DISTANCE OF 182.35 FEET TO A 1 INCH OPEN END PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY EDUARDO S. ZAVALA SHOWN IN DEED BOOK 6577 PAGE 1635 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE N 56°54'44" W A DISTANCE OF 189.19 FEET TO A 1 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY PAULA JOHNSON HONEYCUTT TRACT 2 SHOWN IN DEED BOOK 6599 PAGE 2736 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE N 56°52'34" W A DISTANCE OF 1,063.26 FEET TO THE BASE OF A DISTURBED 1 INCH OPEN END IRON PIPE, SAID PIPE BEING COMMON BETWEEN THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY ELLA S. LANIER SHOWN IN DEED BOOK 4687 PAGE 343 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE N 31°03'31" E A DISTANCE OF 532.50 FEET TO A 3/4 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY TAMMY SUE BISHOP SHOWN IN DEED BOOK 6628 PAGE 103 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 55°18'21" E A DISTANCE OF 68.16 FEET TO A 5/8 IN IRON ROD SET, SAID ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY TAMMY SUE BISHOP PREVIOUSLY DESCRIBED,

THENCE S 55°40'27" E A DISTANCE OF 288.22 FEET TO A 1 INCH PINCHED IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY JAMES MICHAEL HOFFMAN SHOWN IN DEED BOOK 6173 PAGE 2579 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 55°40'27" E A DISTANCE OF 139.82 FEET TO A 5/8 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY JOHN C. LANGLEY SHOWN IN DEED BOOK 1622 PAGE 771 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 55°40'27" E A DISTANCE OF 397.12 FEET TO A 3/4 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING 1.16 FEET NORTHEAST OF THE PROPERTY LINE, SAID PIPE BEING

COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY HARRY S. AND ELDA L. CANNADY SHOWN IN DEED BOOK 6273 PAGE 539 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 55°40'27" E A DISTANCE OF 123.49 FEET TO A 1 AND 1/4 INCH PINCHED IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY CANO HORACIO MENDOZA SHOWN IN DEED BOOK 6574 PAGE 1914 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 55°40'27" E A DISTANCE OF 124.85 FEET TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY SOUTHSIDE HABITAT LLC SHOWN IN DEED BOOK 5699 PAGE 2500 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 55°40'27" E A DISTANCE OF 127.92 FEET TO A 2 AND 1/2 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY ABLE VENTURES LTD SHOWN IN DEED BOOK 2498 PAGE 963 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 32°37'58" W A DISTANCE OF 134.91 FEET TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING CALLED POINT OF BEGINNING 3, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY ABLE VENTURES LTD PREVIOUSLY DESCRIBED,

THENCE S 62°05'03" E A DISTANCE OF 301.61 FEET TO POINT OF BEGINNING 1, TRACT CONTAINING 14.67 ACRES.

LESS AND EXCEPT

THAT CERTAIN TRACT OF LAND BEING IN THE CITY OF WILMINGTON, NEW HANOVER COUNTY, NORTH CAROLINA, SAID TRACT BEING A PORTION OF THE PROPERTY DESCRIBED IN DEED BOOK 6599 PAGE 2736 TRACT 3 OF NEW HANOVER COUNTY REGISTER OF DEEDS AND BEING MORE FULLY DESCRIBED AS FOLLOWS:

COMMENCING AT A CAPPED REBAR SET WITHIN THE RIGHT OF WAY OF ALEXANDER ROAD, SAID CAPPED REBAR HAVING THE NORTH CAROLINA STATE PLANE COORDINATES OF NORTHING: 196,997.05 AND EASTING: 2,359,772.58 IN THE CITY OF WILMINGTON, HARNETT TOWNSHIP, NEW HANOVER COUNTY, NORTH CAROLINA, THENCE N 11°55'41" W A DISTANCE OF 576.07 FEET TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY ABLE VENTURES LTD PREVIOUSLY DESCRIBED, SAID

IRON ROD HAVING THE NORTH CAROLINA STATE PLANE COORDINATES OF NORTHING: 197,560.69 AND EASTING: 2,359,653.51, BEING POINT OF BEGINNING 3.

THENCE FROM POINT OF BEGINNING 3, S 32°37'58" W A DISTANCE OF 30.10 FEET TO A 3/4 INCH IRON ROD FOUND, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY DAMARIS REBOLLO CONTRERAS PREVIOUSLY DESCRIBED,

THENCE S 33°00'48" W A DISTANCE OF 17.23 FEET TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY DAMARIS REBOLLO CONTRERAS PREVIOUSLY DESCRIBED,

THENCE N 57°00'35" W A DISTANCE OF 130.92 FEET TO A 1/2 INCH IRON ROD FOUND, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY PAULA JOHNSON HONEYCUTT TRACT 1 PREVIOUSLY DESCRIBED,

THENCE N 33°36'49" E A DISTANCE OF 185.24 FEET TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY SOUTHSIDE HABITAT LLC PREVIOUSLY DESCRIBED,

THENCE S 55°40'27" E A DISTANCE OF 127.92 FEET TO A 2 AND 1/2 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY ABLE VENTURES LTD PREVIOUSLY DESCRIBED,

THENCE S 32°37'58" W A DISTANCE OF 134.91 FEET TO POINT OF BEGINNING 3, TRACT CONTAINING 0.55 ACRES MORE OR LESS.

**LEGAL DESCRIPTION**

R03600-003-008-001

THAT CERTAIN TRACT OF LAND BEING IN THE CITY OF WILMINGTON, NEW HANOVER COUNTY, NORTH CAROLINA, SAID TRACT BEING A PORTION OF THE PROPERTY DESCRIBED IN DEED BOOK 6599 PAGE 2736 TRACT 2 OF NEW HANOVER COUNTY REGISTER OF DEEDS AND BEING MORE FULLY DESCRIBED AS FOLLOWS:

COMMENCING AT A CAPPED REBAR SET WITHIN THE RIGHT OF WAY OF ALEXANDER ROAD, SAID CAPPED REBAR HAVING THE NORTH CAROLINA STATE PLANE COORDINATES OF NORTHING: 196,997.06 AND EASTING: 2,359,772.58, IN THE CITY OF WILMINGTON, HARNETT TOWNSHIP, NEW HANOVER COUNTY, NORTH CAROLINA, THENCE N 62°24'44" W A DISTANCE OF 46.02 FEET TO A 2 INCH OPEN END IRON PIPE ON THE WESTERLY RIGHT OF WAY LINE OF ALEXANDER ROAD, SAID IRON PIPE BEING COMMON TO THE PROPERTY NOW OR FORMERLY OWNED BY TURTLE TRAIL HOLDINGS, LLC SHOWN IN DEED BOOK 6630 PAGE 716 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS, SAID IRON PIPE HAVING THE NORTH CAROLINA STATE PLANE COORDINATES OF NORTHING: 197,018.37 AND EASTING: 2,359,731.79, SAID IRON PIPE BEING POINT OF BEGINNING 2, THENCE N 64°30'55" W A DISTANCE OF 174.10 FEET TO A 2 INCH OPEN END IRON PIPE, SAID PIPE BEING COMMON TO THE PROPERTY NOW OR FORMERLY OWNED BY EDUARDO S. ZAVALA SHOWN IN DEED BOOK 6577 PAGE 1635 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS, THENCE N 60°29'40" W A DISTANCE OF 190.11 FEET TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY TURTLE TRAIL HOLDINGS, LLC PREVIOUSLY DESCRIBED, SAID IRON ROD BEING THE POINT OF BEGINNING FOR THE TRACT HEREIN DESCRIBED.

THENCE FROM THE POINT OF BEGINNING, S 33°11'27" W A DISTANCE OF 34.49 FEET TO A 5/8 IN IRON ROD SET, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY COASTAL COMMUNITY BAPTIST CHURCH SHOWN IN DEED BOOK 5596 PAGE 1566 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE N 55°06'59" W A DISTANCE OF 921.63 FEET TO THE BASE OF A LEANING 2 AND 1/2 INCH IRON PIPE FOUND, PASSING OVER A LEANING 1 AND 1/2 INCH IRON PIPE FOUND AT A DISTANCE OF 346.39 FEET, SAID 2 AND 1/2 INCH IRON PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY COASTAL COMMUNITY BAPTIST CHURCH PREVIOUSLY DESCRIBED,

THENCE N 65°10'31" W A DISTANCE OF 328.03 FEET TO A 1 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY OGDEN PINES HOA INC SHOWN IN DEED BOOK 3147 PAGE 917 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE N 30°54'03" E A DISTANCE OF 130.01 FEET TO THE BASE OF A DISTURBED 1 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY PAULA JOHNSON HONEYCUTT TRACT 1 SHOWN IN DEED BOOK 6599 PAGE 2736 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 56°52'34" E A DISTANCE OF 1,063.26 FEET TO A 1 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY EDUARDO S. ZAVALA PREVIOUSLY DESCRIBED,

THENCE S 34°36'20" W A DISTANCE OF 76.30 FEET TO A 1 INCH REBAR FOUND, SAID REBAR BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY EDUARDO S. ZAVALA PREVIOUSLY DESCRIBED,

THENCE S 56°50'43" E A DISTANCE OF 189.58 FEET TO THE POINT OF BEGINNING FOR THIS TRACT, TRACT CONTAINING 2.51 ACRES MORE OR LESS.



**LEGAL DESCRIPTION**  
**R03600-003-117-000**

THAT CERTAIN TRACT OF LAND BEING IN THE CITY OF WILMINGTON, NEW HANOVER COUNTY, NORTH CAROLINA, SAID TRACT BEING A PORTION OF THE PROPERTY DESCRIBED IN DEED BOOK 6599 PAGE 2736 TRACT 3 OF NEW HANOVER COUNTY REGISTER OF DEEDS AND BEING MORE FULLY DESCRIBED AS FOLLOWS:

COMMENCING AT A CAPPED REBAR SET WITHIN THE RIGHT OF WAY OF ALEXANDER ROAD, SAID CAPPED REBAR HAVING THE NORTH CAROLINA STATE PLANE COORDINATES OF NORTHING: 196,997.06 AND EASTING: 2,359,772.58 IN THE CITY OF WILMINGTON, HARNETT TOWNSHIP, NEW HANOVER COUNTY, NORTH CAROLINA, THENCE N 11°55'41" W A DISTANCE OF 576.07 FEET TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY ABLE VENTURES LTD SHOWN IN DEED BOOK 2498 PAGE 963 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS, SAID IRON ROD HAVING THE NORTH CAROLINA STATE PLANE COORDINATES OF NORTHING: 197,560.69 AND EASTING: 2,359,653.51, BEING POINT OF BEGINNING 3.

THENCE FROM POINT OF BEGINNING 3, S 32°37'58" W A DISTANCE OF 30.10 FEET TO A 3/4 INCH IRON ROD FOUND, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY DAMARIS REBOLLO CONTRERAS SHOWN IN DEED BOOK 9801 PAGE 2516 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 33°00'48" W A DISTANCE OF 17.23 FEET TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY PAULA JOHNSON HONEYCUTT TRACT 1 SHOWN IN DEED BOOK 6599 PAGE 2736 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE N 57°00'35" W A DISTANCE OF 130.92 FEET TO A 1/2 INCH IRON ROD FOUND, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY PAULA JOHNSON HONEYCUTT TRACT 1 PREVIOUSLY DESCRIBED,

THENCE N 33°36'49" E A DISTANCE OF 185.24 FEET TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW

OR FORMERLY OWNED BY SOUTHSIDE HABITAT LLC SHOWN IN DEED BOOK 5699 PAGE 2500 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 55°40'27" E A DISTANCE OF 127.92 FEET TO A 2 AND 1/2 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY ABLE VENTURES LTD PREVIOUSLY DESCRIBED,

THENCE S 32°37'58" W A DISTANCE OF 134.91 FEET TO POINT OF BEGINNING 3, TRACT CONTAINING 0.55 ACRES MORE OR LESS.

ALONG WITH A 30 FOOT ACCESS EASEMENT AS DESCRIBED IN DEED BOOK 4087 PAGE 739 CROSSING THE HERETOFORE DESCRIBED TRACT TO BENEFIT THE ADJOURNER PROPERTY NOW OR FORMERLY OWNED BY PAULA JOHNSON HONEYCUTT TRACT 1 PREVIOUSLY DESCRIBED.

**LEGAL DESCRIPTION**  
R03600-003-008-000

THAT CERTAIN TRACT OF LAND BEING IN THE CITY OF WILMINGTON, NEW HANOVER COUNTY, NORTH CAROLINA, SAID TRACT BEING A PORTION OF THE PROPERTY DESCRIBED IN DEED BOOK 6630 PAGE 716 OF NEW HANOVER COUNTY REGISTER OF DEEDS AND BEING MORE FULLY DESCRIBED AS FOLLOWS:

COMMENCING AT A CAPPED REBAR SET WITHIN THE RIGHT OF WAY OF ALEXANDER ROAD, SAID CAPPED REBAR HAVING THE NORTH CAROLINA STATE PLANE COORDINATES OF NORTHING: 196,997.06 AND EASTING: 2,359,772.58, IN THE CITY OF WILMINGTON, HARNETT TOWNSHIP, NEW HANOVER COUNTY, NORTH CAROLINA, THENCE N 62°24'44" W A DISTANCE OF 46.02 FEET TO A 2 INCH OPEN END IRON PIPE ON THE WESTERLY RIGHT OF WAY LINE OF ALEXANDER ROAD, SAID IRON PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY EDUARDO S. ZAVALA SHOWN IN DEED BOOK 6577 PAGE 1635 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS, SAID IRON PIPE HAVING THE NORTH CAROLINA STATE PLANE COORDINATES OF NORTHING: 197,018.37 AND EASTING: 2,359,731.79, BEING POINT OF BEGINNING 2.

THENCE FROM POINT OF BEGINNING 2, ALONG THE RIGHT OF WAY OF ALEXANDER ROAD AND ALONG THE ARCH OF A CURVE TO THE LEFT A DISTANCE OF 93.12 FEET, SAID CURVE HAVING A RADIUS OF 126.40 FEET AND A CHORD BEARING AND DISTANCE OF S 07°21'30" W 91.03 FEET, TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING ON THE WESTERLY RIGHT OF WAY LINE OF ALEXANDER ROAD, AND COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY COASTAL COMMUNITY BAPTIST CHURCH SHOWN IN DEED BOOK 5596 PAGE 1566 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE N 55°06'59" W A DISTANCE OF 402.08 FEET TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY PAULA JOHNSON HONEYCUTT TRACT 2 SHOWN IN DEED BOOK 6599 PAGE 2736 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE N 33°11'27" E A DISTANCE OF 34.49 FEET TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY EDUARDO S. ZAVALA PREVIOUSLY DESCRIBED,

**LEGAL DESCRIPTION**

**R03600-003-009-001**

THAT CERTAIN TRACT OF LAND BEING IN THE CITY OF WILMINGTON, NEW HANOVER COUNTY, NORTH CAROLINA, SAID TRACT BEING A PORTION OF THE PROPERTY DESCRIBED IN DEED BOOK 6577 PAGE 1635 OF NEW HANOVER COUNTY REGISTER OF DEEDS AND BEING MORE FULLY DESCRIBED AS FOLLOWS:

COMMENCING AT A CAPPED REBAR SET WITHIN THE RIGHT OF WAY OF ALEXANDER ROAD, SAID CAPPED REBAR HAVING THE NORTH CAROLINA STATE PLANE COORDINATES OF NORTHING: 196,997.06 AND EASTING: 2,359,772.58, IN THE CITY OF WILMINGTON, HARNETT TOWNSHIP, NEW HANOVER COUNTY, NORTH CAROLINA, THENCE, N 62°24'44" W A DISTANCE OF 46.02 FEET TO A 2 INCH OPEN END IRON PIPE ON THE WESTERLY RIGHT OF WAY LINE OF ALEXANDER ROAD, SAID IRON PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY TURTLE TRAIL HOLDINGS, LLC SHOWN IN DEED BOOK 6630 PAGE 716 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS, SAID IRON PIPE HAVING THE NORTH CAROLINA STATE PLANE COORDINATES OF NORTHING: 197,018.37 AND EASTING: 2,359,731.79, BEING POINT OF BEGINNING 2.

THENCE FROM POINT OF BEGINNING 2, N 64°30'55" W A DISTANCE OF 174.10 FEET TO A 2 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY TURTLE TRAIL HOLDINGS, LLC PREVIOUSLY DESCRIBED,

THENCE N 60°29'40" W A DISTANCE OF 190.11 FEET TO A 5/8 INCH IRON ROD SET, SAID IRON ROD BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY PAULA JOHNSON HONEYCUTT TRACT 2 SHOWN IN DEED BOOK 6599 PAGE 2736 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE N 56°50'43" W A DISTANCE OF 189.58 FEET TO A 1 INCH REBAR FOUND, SAID REBAR BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY PAULA JOHNSON HONEYCUTT TRACT 2 PREVIOUSLY DESCRIBED,

THENCE N 34°36'20" E A DISTANCE OF 76.30 FEET TO A 1 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY PAULA JOHNSON HONEYCUTT TRACT 1

SHOWN IN DEED BOOK 6599 PAGE 2736 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 56°54'44" E A DISTANCE OF 189.19 FEET TO A 1 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY DAMARIS CONTRERAS ET AL. SHOWN IN DEED BOOK 5968 PAGE 1386 AS RECORDED IN THE NEW HANOVER COUNTY REGISTER OF DEEDS,

THENCE S 56°57'24" E A DISTANCE OF 52.21 FEET TO A 1 AND 1/4 INCH PINCHED IRON PIPE FOUND, SAID PIPE BEING COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY DAMARIS CONTRERAS ET AL. PREVIOUSLY DESCRIBED,

THENCE S 65°33'29" E A DISTANCE OF 298.96 FEET TO THE BASE OF A DISTURBED 5/8 INCH OPEN END IRON PIPE FOUND, SAID PIPE BEING ON THE WESTERLY RIGHT OF WAY OF ALEXANDER ROAD, AND COMMON TO THE PROPERTY DESCRIBED HEREIN AND THE PROPERTY NOW OR FORMERLY OWNED BY DAMARIS CONTRERAS ET AL. PREVIOUSLY DESCRIBED,

THENCE ALONG THE RIGHT OF WAY OF ALEXANDER ROAD S 24°37'06" W A DISTANCE OF 87.52 FEET TO THE POINT OF BEGINNING 2, TRACT CONTAINING 1.00 ACRE MORE OR LESS.

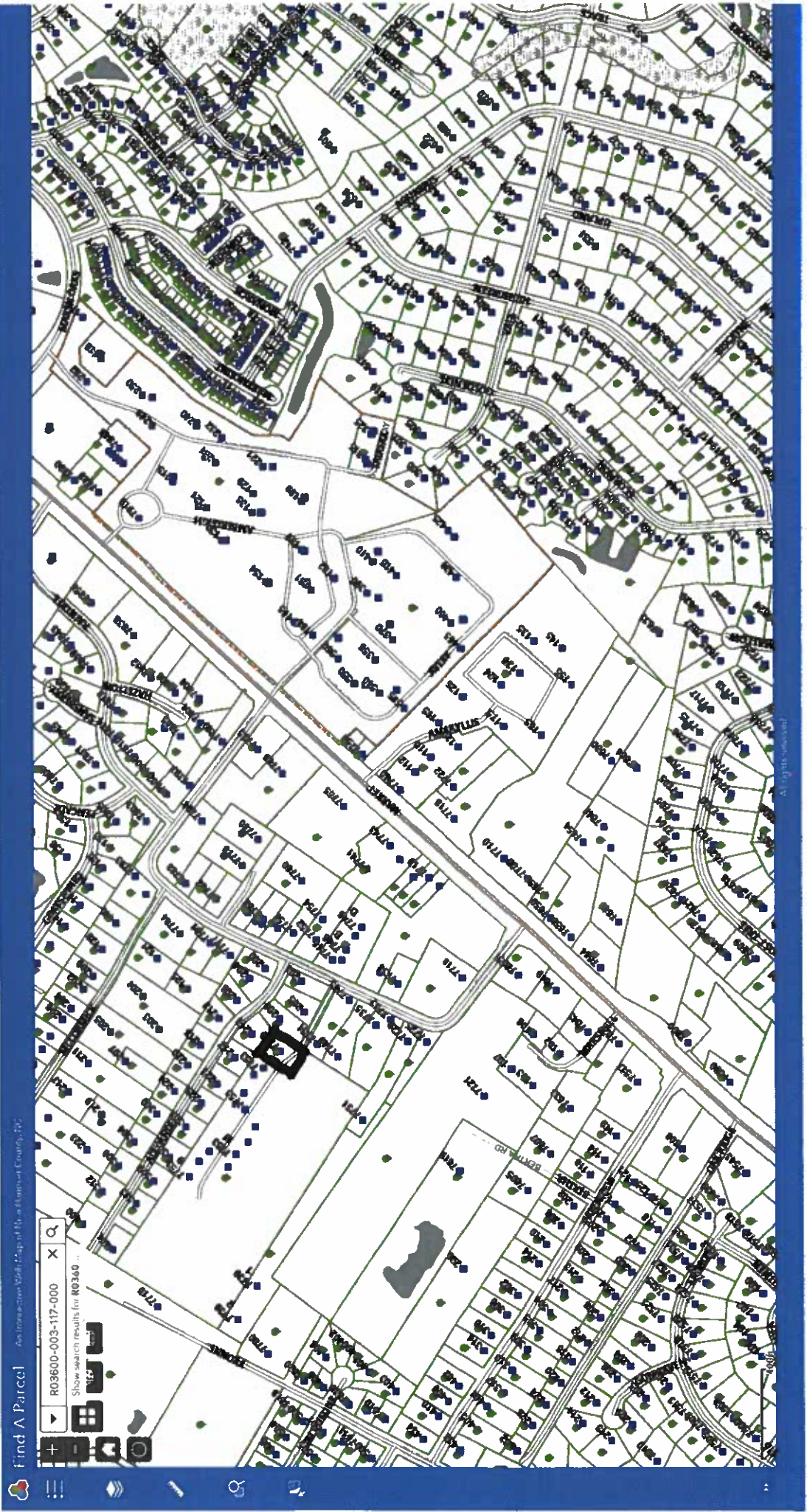
## ADJACENT PROPERTY OWNERS

PARID	PHYSICAL ADDRESS	OWNER	MAILING ADDRESS	CITY	STATE	ZIP
R03614-009-005-000	217 ALEXANDER PL	DAUGHTRY SAMUEL G	102 LIVE OAK LN	WILMINGTON	NC	28411
R03600-003-198-000	7735 ALEXANDER RD	CONTRERAS DAMARIS C RAUL C TEPATE	7733 ALEXANDER RD	WILMINGTON	NC	28411
R03614-009-008-000	216 ALEXANDER PL	JARQUIN	216 ALEXANDER PL	WILMINGTON	NC	28411
R03600-003-117-000	7759 ALEXANDER RD	HONEYCUTT PAULA JOHNSON	206 HEADWATERS DR 10300 LURIA COMMONS CT	HAMPSTEAD	NC	28443
R03610-003-003-000	309 ALEXANDER PL	METAKES KAREN TRUSTEE	APT 2E	BURKE	VA	22015
R03610-003-010-000	300 ALEXANDER PL	GRAF MICHELLE A AUSTIN	4 BALLANTRAE CT	STAFFORD	VA	22554
R03600-003-110-000	207 MCCORMICK LN	NEWTON FRANKLIN K JR	205 MCCORMICK LN	WILMINGTON	NC	28411
R03610-003-011-000	224 ALEXANDER PL	GARRELL KIMBERLY	224 ALEXANDER PL	WILMINGTON	NC	28411
R03610-003-002-000	305 ALEXANDER PL	LANGLEY JOHN C	923 DAISY LEE DR	WILMINGTON	NC	28411
R03600-003-009-001	7729 ALEXANDER RD	ZAVALA EDUARDO S	7729 ALEXANDER RD	WILMINGTON	NC	28411
R03610-003-012-000	220 ALEXANDER PL	BISETTE PARKER BENNETT	409 MERWIN RD	RALEIGH	NC	27606
R03600-003-017-005	231 MCCORMICK LN	GORE STEPHEN	320 ISLAND CREEK DR	WILMINGTON	NC	28411
R03610-003-004-000	313 ALEXANDER PL	HOFFMAN JAMES MICHAEL	313 ALEXANDER PL	WILMINGTON	NC	28405
R03610-003-007-000	312 ALEXANDER PL	WINSLOW TAMMY C	437 VALLEYMEADE DR	KERNERSVILLE	NC	27284
R03614-009-006-000	221 ALEXANDER PL	CANNADY HARRY S ELDA L SHEPARD BEVERLY S	221 ALEXANDER PL	WILMINGTON	NC	28411
R03600-003-017-007	201 MCCORMICK LN	REVOCABLE TRUST ETAL	201 MCCORMICK LN	WILMINGTON	NC	28411
R03600-003-017-004	225 MCCORMICK LN	FREEMAN CRYSTAL Y ASHLEY B	102 KINGSPORT DR	HAMPSTEAD	NC	28443
R03610-003-008-000	308 ALEXANDER PL	WINSLOW TAMMY C	437 VALLEYMEADE DR 129 MIDDLE SOUND LOOP RD	KERNERSVILLE	NC	27284
R03610-003-009-000	304 ALEXANDER PL	COVIL SHIRLEY A ETAL	RD	WILMINGTON	NC	28411
R03600-003-017-001	211 MCCORMICK LN	GIEDEMANN THEODORE M	211 MCCORMICK LN	WILMINGTON	NC	28411
R03600-003-109-000	125 MCCORMICK LN	VILLASENOR CASTRO MIGUEL A	125 MCCORMICK LN	WILMINGTON	NC	28411
R03614-009-002-000	205 ALEXANDER PL	MERA DEVANIRA ETAL	205 ALEXANDER PL	WILMINGTON	NC	28411
R03614-009-003-000	209 ALEXANDER PL	ABLE VENTURES LTD	1230 MAYNARD RD NE	CARY	NC	27513
R03614-009-004-000	213 ALEXANDER PL	SOUTHSIDE HABITAT LLC	PO BOX 522	WHITEVILLE	NC	28472
R03610-003-005-000	401 ALEXANDER PL	HUNTWORX RICHARD A	401 ALEXANDER PL	WILMINGTON	NC	28411
R03614-009-001-000	201 ALEXANDER PL	MENDOZA JULIO R	3808 WRIGHTSVILLE AVE	WILMINGTON	NC	28403

R03614-009-012-000	200 ALEXANDER PL	JACKSON CATHERINE GARY	2070 BIGGERS LAKE RD, #5	SHELBY	NC	28152
R03600-003-017-006	205 MCCORMICK LN	NEWTON FRANKLIN KEVIN JR	205 MCCORMICK LN	WILMINGTON	NC	28411
R03600-003-185-000	219 MCCORMICK LN	GARRETT JERRY W II HEIRS ETAL	219 MCCORMICK LN	WILMINGTON	NC	28411
R03614-009-010-000	208 ALEXANDER PL	BENTON MICHAEL A PEGGY ANN	208 ALEXANDER PL	WILMINGTON	NC	28405
R03614-009-011-000	204 ALEXANDER PL	YUNASKA ROBERT	102 SEAPATH EST	WRIGHTSVILL	NC	28480
R03614-009-007-000	225 ALEXANDER PL	ADAMS JOHN DEAN CAROL JEAN	225 ALEXANDER PL	E BEACH	NC	28405
R03614-009-009-000	212 ALEXANDER PL	GRAF JOHN C MICHELLE A	4 BALLANTRAE CT	STAFFORD	VA	22554
R03610-003-006-000	400 ALEXANDER PL	COVIL SHIRLEY A ETAL	129 MIDDLE SOUND LOOP	WILMINGTON	NC	28411
R03610-003-001-000	301 ALEXANDER PL	MORAN MICHAEL H PATRICIA D	301 ALEXANDER PL	WILMINGTON	NC	28405
R03600-003-017-003	221 MCCORMICK LN	FREEMAN CRYSTAL Y ASHLEY B	102 KINGSPORT DR	HAMPSTEAD	NC	28443
R03600-003-197-001	7745 ALEXANDER RD	CONTRERAS DAMARIS REBOLLO	7733 ALEXANDER RD	WILMINGTON	NC	28411
R03600-004-020-000	7746 ALEXANDER RD	KRIEGER CHARLES W PATRICIA A	3209 ASTER CT	WILMINGTON	NC	28409
R03600-004-019-000	7752 ALEXANDER RD	TRUSTEES	1219 MIDDLE SOUND LOOP	WILMINGTON	NC	28405
R03600-004-015-000	7760 ALEXANDER RD	PETERSON G W LINDA	1219 MIDDLE SOUND LOOP	WILMINGTON	NC	28405
R03614-001-043-000	7700 YVONNE RD	OGDEN PINES HOA INC	RD	ORIENTAL	NC	28571
R03600-003-008-000	7725 ALEXANDER RD	TURTLE TRAIL HOLDING LLC	PO BOX 399	WILMINGTON	NC	28412
R03614-001-042-000	7620 YVONNE RD	HOWARD CAROLYN J	4026 CHAPRA DR	WILMINGTON	NC	28411
R03600-003-008-001		HONEYCUTT PAULA JOHNSON	517 DIANE DR	WILMINGTON	NC	28443
R03614-001-039-000	402 JACQUELINE DR	BEGLEY DAVID NORBERT	206 HEADWATER DR	HAMPSTEAD	NC	28443
R03600-003-007-000	7721 ALEXANDER RD	COASTAL COMMUNITY BAPTIST	402 JACQUELINE DR	WILMINGTON	NC	28411
R03600-003-007-001	7619 BERTHA RD	CHURCH	PO BOX 12270	WILMINGTON	NC	28405
R03614-001-040-000	404 JACQUELINE DR	SMITH WALTER STEVEN	PO BOX 12270	WILMINGTON	NC	28405
R03600-003-193-000	107 MARSHALL CT	SCF RC FUNDING IV LLC	404 JACQUELINE DR	WILMINGTON	NC	28405
			902 CARNEGIE CENTER	WILMINGTON	NC	28405
			BLVD	WILMINGTON	NC	28405
			SUITE 520	WILMINGTON	NC	28405
				PRINCETON	NJ	08540

R03600-003-011-000	7725 YVONNE RD	BUFFALO ASSOCIATES LLC ET	6131 FALLS OF NEUSE RD	RALEIGH	NC	27609
R03600-003-114-000	7710 YVONNE RD	AL	SUITE 200	VALDEZ	AK	99686
R03600-004-021-000	7734 ALEXANDER RD	BRAGG MILLER	PO BOX 121	WILMINGTON	NC	28411
R03600-004-022-000	7718 ALEXANDER RD	BROWN EDWARD LAMAR	7718 ALEXANDER RD	WILMINGTON	NC	28411
R03600-004-001-000		BROWN JOHNNY E	7718 ALEXANDER RD	WILMINGTON	NC	28411
R03600-003-009-002	7753 ALEXANDER RD	HONEYCUTT PAULA J	206 HEADWATER DR	HAMPSTEAD	NC	28443
R03600-003-010-000		BUFFALOE ASSOCIATES LLC ET	6131 FALLS OF NEUSE RD	RALEIGH	NC	27609
R03600-003-018-000	7588 PLANTATION RD	AL	SUITE 200	SPRING LAKE	NC	28390
R03600-003-197-000	7745 ALEXANDER RD	CESSNA TINA MUSSELMAN	130 SPRUCE HOLLOW CIR	WILMINGTON	NC	28411
		CONTRERAS DAMARIS REBOLLO	7733 ALEXANDER RD			





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R03600-003-117-000

Show search results for R03600...







Find A Parcel

An Interactive Web Map of City, Township, County, DC

R03600-003-008-000 X

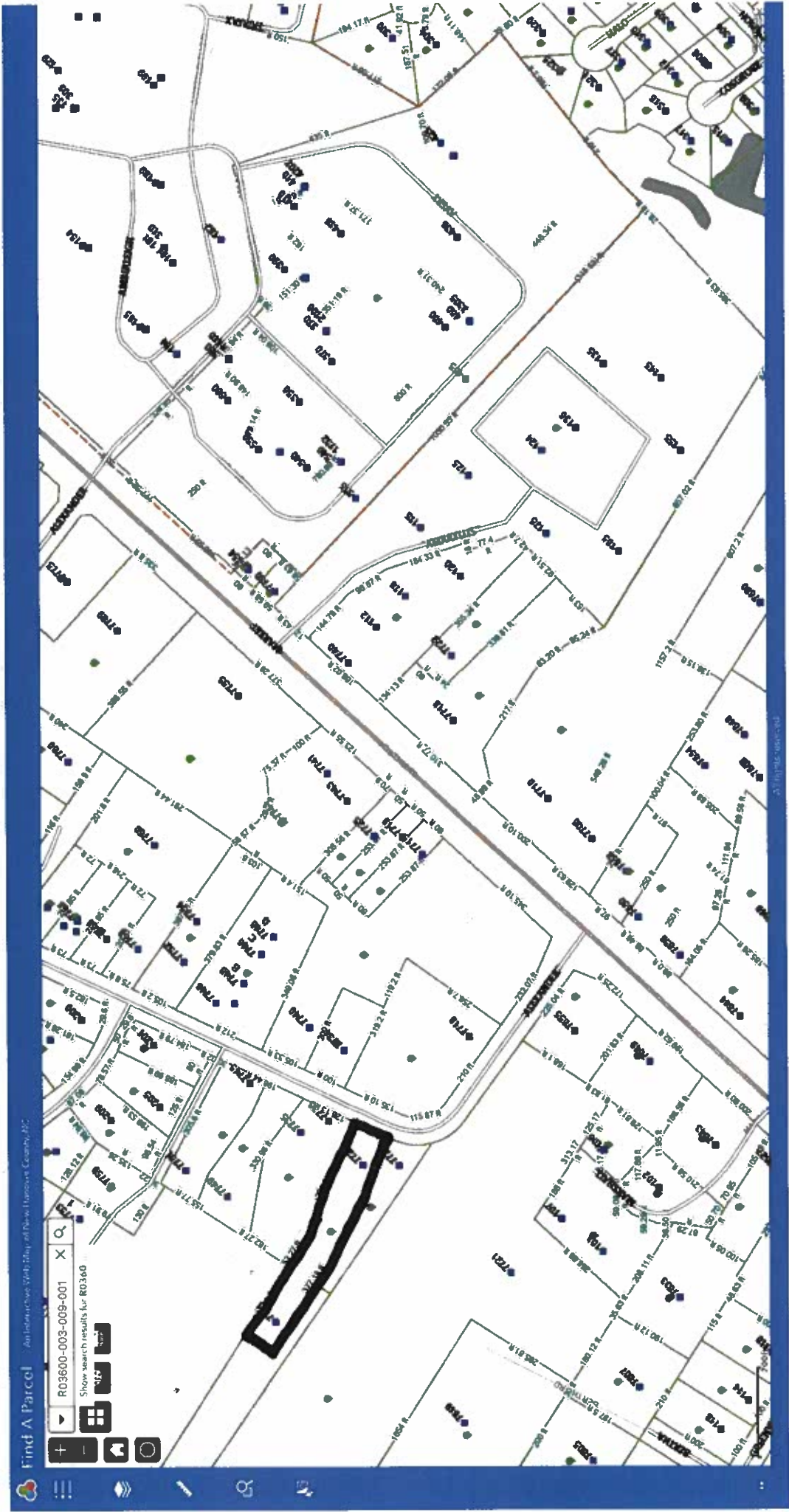
Show search results for R03600



R03600-003-008-001 X Q  
Show search results for R03600







Find A Parcel An Interactive Web-Tool of Davis Landscape County, NC

R03600-003-009-001

Show search results for R03600



Search results for R0360

R0360-003-009-002

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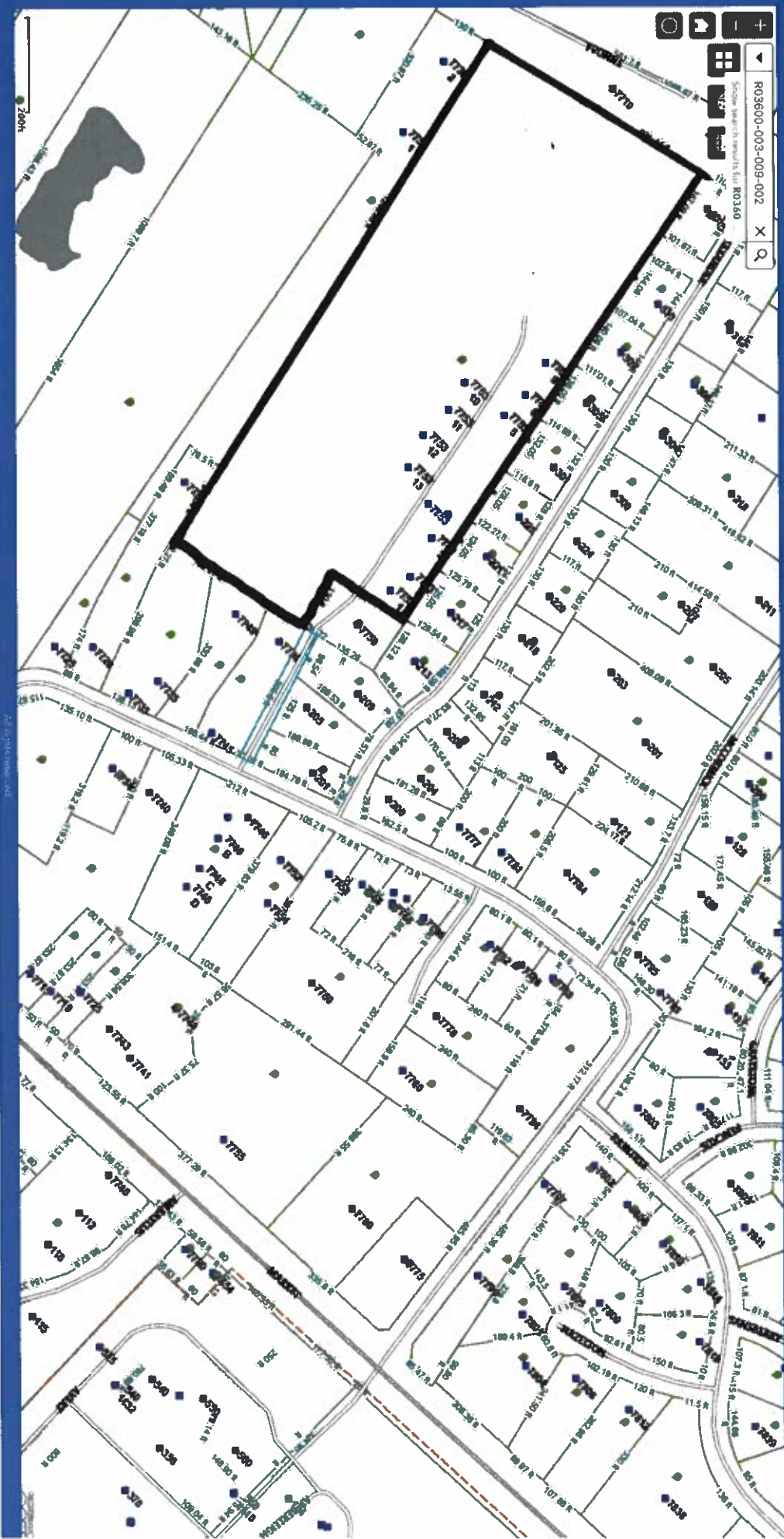
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# WILMINGTON URBAN AREA METROPOLITAN PLANNING ORGANIZATION

**P.O. Box 1810  
Wilmington, North Carolina 28402  
910 341 3258 910 341 7801 FAX**

## Traffic Impact Analysis (TIA): Procedures and Scoping Checklist

Date Submitted: 4 / 17 / 2023

*(To be filled out by applicant)*

Date Received by WMPO or NCDOT:    /   /   

*(To be filled out by the Department)*

The purpose of this checklist is to outline and streamline the Traffic Impact Analysis (TIA) scoping process and provide scoping information for WMPO to review and comment prior to developing a TIA.

The review procedures are as follows:

- 1** Contact the Wilmington Metropolitan Planning Organization (WMPO) to determine if a TIA will be required for the development you are proposing within the WMPO Planning Area Boundary. If a TIA is required, the WMPO will coordinate scoping with the Applicant, NCDOT and the governing planning department.
- 2** Also, if a TIA is required, the Applicants Traffic Engineer shall fill out the checklist below in its entirety in as much detail as possible, include all exhibits and documentation as noted in the checklist and provide it to the WMPO representative no later than 7 business days prior to the date of the next scoping meeting. Scoping meetings occur each Wednesday.
- 3** The Applicant's Traffic Engineer (and other interested representatives from the Applicants Design Team) may attend the scoping meeting as deemed necessary and by WMPO request only.
- 4** Once the scope has been established and submitted, the WMPO will coordinate with NCDOT and the governing Planning department to finalize and submit a formal **Scoping Letter** to the Transportation Engineer (applicant). If the checklist is deemed complete and in sufficient detail, the scoping letter will be issued within 10 business days of the date of the official receipt date.
- 5** TIA Submittal: The Applicant shall submit 4 hardcopies and Digital Copy of the TIA and associated models and files to the WMPO for distribution to NCDOT and governing planning agency for review. The TIA preliminary review comments will be sent within 20 business days of TIA submittal. The 20-day review clock is re-set with each set of comments to the Applicant. The TIA is not considered as final and approved until the **TIA Approval Letter** is submitted by the WMPO.

Development Name: Abberly Wilmington

WMPO ID # \_\_\_\_\_

Updated 5/23/18



**TIA Scoping Checklist:**

1. Engineering Firm of Record: Ramey Kemp Associates
  
2. Contact information:
  - a. Applicant Name: Chase Smith
  - b. Email: csmith@rameykemp.com
  - c. Address: 5808 Faringdon Place, Suite 100
  - d. Phone Number: 919-872-5115
  
3. Development Name: Abberly Wilmington
  
4. Tax Parcel ID #: PID: R03600-003-009-002, R03600-003-008-001, R03600-003-009-001  
R03600-003-008-000, R03600-003-117-000
  
5. Site Address(s):
 

7753 Alexander Road Wilmington, NC

See attached site location map
  
6. Type of Development: List Land Use(s)/Intensity and ITE codes:
 

316 Mid-Rise apartments (221)

---
  
7. Development Concept: see attached
  - a. Provide a site plan or conceptual plan (include road circulation pattern and development access points to public rights of way).
  - b. Add scaled site plan on Arial imagery which includes adjacent properties and access points
  
8. If this development is intended to be built in multiple phases the TIA is required to accurately reflect the phasing. If the phasing is not included within the TIA, a revision and/or interim TIA's will be required.
  - a. What is the proposed build out year? 2027
  - b. If the project is phased, specify the build out year for each phase.  
N/A

Development Name: Abberly Wilmington  
WMPO ID #: \_\_\_\_\_



9. Site Trip Generation:

- a. Provide a trip generation estimate using the ITE Trip Generation Manual (latest) by phase, if applicable. See attached.
- b. Provide pass-by and internal capture reduction calculations, if applicable.
- c. Provide a trip generation on AM / PM and Saturday peak hour.

N/A

10. Check to see if any of the following apply:

- a. Proposed TIA is within the area of a known Transportation Improvement Project (TIP)  
Yes, U-4902D - under construction

- b. NCDOT STIP Map

(<http://ncdot.maps.arcgis.com/home/webmap/viewer.html?webmap=cb02f4f828974670ad01bb83be91b18c>)

N/A

- c. Proposed TIA is within a surrounding municipality that has a planned or active transportation projects

N/A

11. Approved Adjacent Developments can be found at the following link:

- a. List all approved adjacent developments within the TIA study area:

WMPO TIA WebApp

(<http://wilmingtonnc.maps.arcgis.com/apps/webappviewer/index.html?id=d3af4a0e091941d8886344c4d1c609d5>)

Amberleigh Shores Phase II, Bailey Shoppes on Market Development, Bayshore Commercial Development

- b. Additional developments may be added to the approved scope once reviewed.

12. Roadway and/or Intersection improvements (planned and funded or required by others based on an approved TIA) within the general study area. List the proposed Intersections to be studied.

Attached a map with a list that includes streets and intersections labeled as described below.

13. Streets and intersections should include State Road Designation (US or State Road Number US/SR #, followed by the local road name in parenthesis. E.g.:

Market Street should be denoted as US-17-BUS (Market Street).

Judges Rd should be denoted as SR-2127 (Judges Rd).

State Road designations can be found via the following link: NCDOT State Maintained Network Map

(<http://ncdot.maps.arcgis.com/home/webmap/viewer.html?webmap=5d3ad78971714a30be7ff97fd580e4d5>)

- a. US 17 & SR 1345 (Alexander Road) - Northern Connection
- b. US 17 & SR 1345 (Alexander Road) - Southern Connection
- c. \_\_\_\_\_
- d. \_\_\_\_\_

Development Name: Abberly Wilmington

WMPO ID #: \_\_\_\_\_

Updated 5/23/18





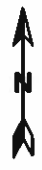
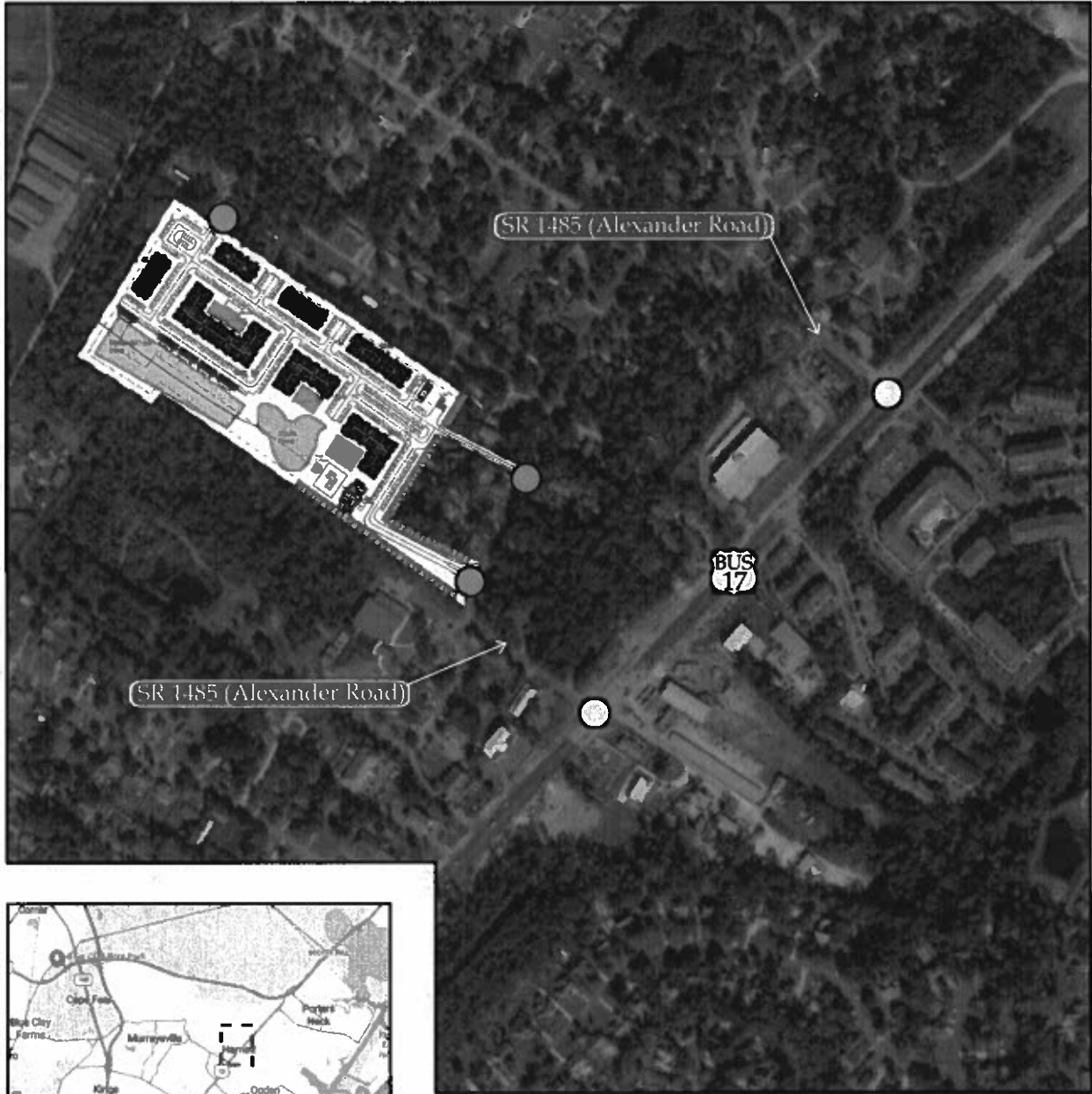
- 14. Growth Factor:  1.5% % (provide supporting methodology/reasoning)  
ADT Comparison - see attached.
- 15. Hours of Study and Data Collection to be determined once the scoping documents is reviewed.  
Collect new counts at study intersections during weekday [7-9 AM & 4-6PM]
- 16. Methods of Study as outlined in the NCDOT Congestion Management Capacity Analysis Guidelines. (<https://connect.ncdot.gov/resources/safety/Pages/Congestion-Management.aspx>)
- 17. Include Traffic Signal Warrant Analysis for locations with proposed signals. (13 hours counts must be provided.)

Proposed Signal Locations:

- a. N/A
- b. \_\_\_\_\_
- c. \_\_\_\_\_
- d. \_\_\_\_\_
- e. \_\_\_\_\_
- f. \_\_\_\_\_

18. Note:

- a. The TIA report shall be prepared following NCDOT Congestion Management guidelines and signed and sealed by a Professional Engineer. Any deviations must be approved prior to submitting the TIA. Failure to do so will result in an invalid submittal.  
NCDOT Congestion Management Capacity Analysis Guidelines.  
(<https://connect.ncdot.gov/resources/safety/Pages/Congestion-Management.aspx>)
- b. Any improvements shown as needed for the background no-build condition SHALL reflect programmed and funded State/Municipal projects or those required as mitigation for surrounding approved developments. Should improvements outside those listed above be included, the TIA is considered INVALID and will not be returned for resubmission.
- c. If any changes occur (including but not limited to; land use, intensity and/or site access/configuration) additional scoping and analysis may be required.
- d. Data collected or analysis performed prior to an official scoping letter is issued may be considered invalid.
- e. TIA approved scoping letter remain valid for three months from the date of issuance.



**LEGEND**

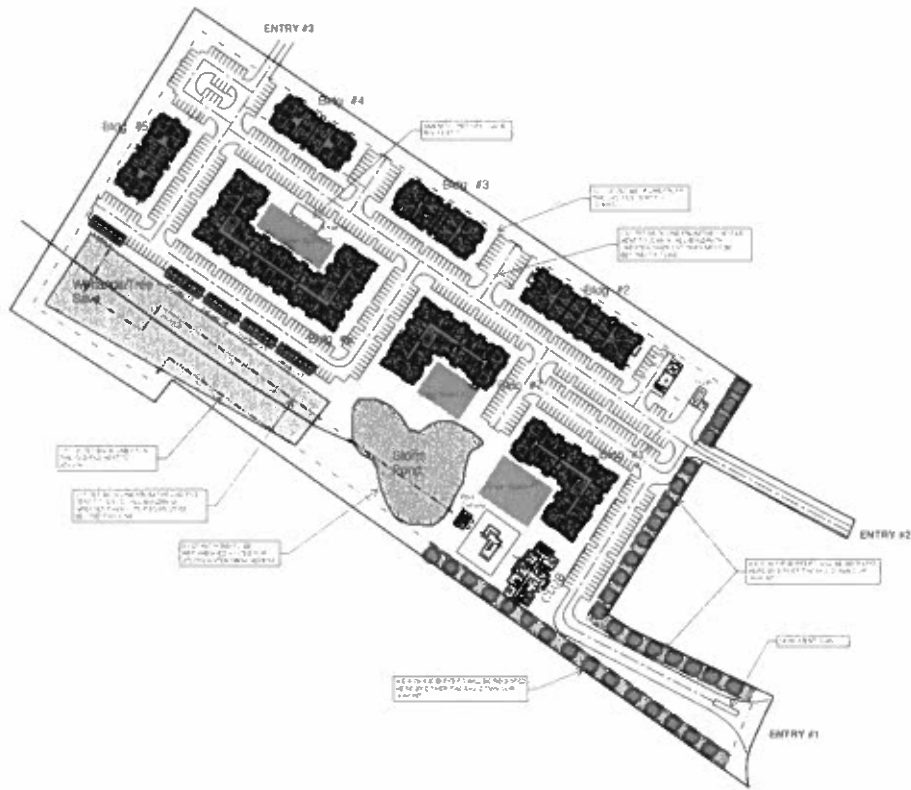
- Study Intersection
- Proposed Site Access
- Study Area



Abberly Wilmington  
Wilmington, NC

Site Location Map

Scale: Not to Scale	Figure 1
---------------------	----------



**JDAVIS >** DESIGN THAT CONNECTS

## Abberly Wilmington- Site Plan

3/17/2023

**Table 1: Site Trip Generation**

<b>TOTAL TRIPS</b>												
Code	Land Use	Size	Unit	Daily			AM Peak			PM Peak		
				In	Out	Total	In	Out	Total	In	Out	Total
221	Mid-Rise Apartments	-316	DU	731	731	1,462	29	98	127	75	49	124
220	Low-Rise	253	DU	849	849	1,698	24	77	101	81	48	129

Year	Road Name: SR 1345 (Alexander Rd) Route ID: 40001345065 Station #: 0650000216	Road Name: US 17 BUS Route ID: 20000017065 Station #: 0650000009	Road Name: SR 2048 (Gordon Rd) Route ID: 40002048065 Station #: 0650000284	Road Name: SR 1409 (Military Cutoff Rd) Route ID: 20000017065 Station #: 0650000391
2021		42,500	25,000	30,500
2020		38,500	22,500	
2019	850	44,500	25,000	29,000
2018				
2017	810	42,000	24,000	28,000
2016				
2015	730		26,000	28,000
2014				
2013	730	32,000	24,000	25,000
2012				
2011	750	36,000	23,000	26,000

2011-19	1.58%			
2011-21		1.67%		
2011-21			0.84%	
2011-21				1.61%

1.42%	1.5%
Average between stations	Recommended Growth Rate



# TRAFFIC IMPACT ANALYSIS

FOR

## ABBERLY WILMINGTON (aka ABBERLY LANDING)

LOCATED

IN

**WILMINGTON, NC**

Prepared For:

HHHunt  
1 Fenton Main Street, Suite 280  
Raleigh, NC 27511



MARCH 2024s

Prepared By: CDS

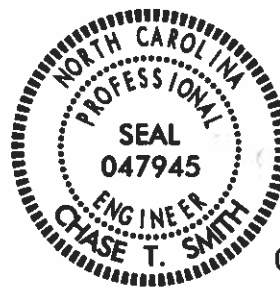
Reviewed By: CTS

**TRAFFIC IMPACT  
ANALYSIS  
FOR  
ABBERLY WILMINGTON  
(aka ABBERLY LANDING)**

**LOCATED IN  
WILMINGTON, NC**

**Prepared For:**  
HHHunt  
1 Fenton Main Street  
Raleigh, NC 27511

**Prepared By:**  
DRMP, Inc.  
License #F-1524



03-06-24

*Chase T. Smith*

# **TRAFFIC IMPACT ANALYSIS ABBERLY WILMINGTON**

**Wilmington, North Carolina**

## **EXECUTIVE SUMMARY**

### **1. Development Overview**

A Traffic Impact Analysis (TIA) was conducted for the proposed Abberly Wilmington residential development in accordance with the Wilmington (City) Unified Development Ordinance (UDO) and North Carolina Department of Transportation (NCDOT) capacity analysis guidelines. The Abberly Wilmington development is proposed to be located along Alexander Road in Wilmington, North Carolina. The proposed development, anticipated to be completed in 2027, is assumed to consist of 253 low-rise apartments. Site access to the parcel is proposed via two full movement driveways along Alexander Road. An additional emergency access (for emergency vehicles only) is proposed near the rear of the development along Alexander Place.

### **2. Existing Traffic Conditions**

The study area for the TIA was determined through coordination with the North Carolina Department of Transportation (NCDOT) and the Wilmington Metropolitan Planning Organization (WMPO) and consists of the following existing intersections:

- US-17 and SR 1345 (Alexander Road) – Northern Connection
- US-17 and SR 1345 (Alexander Road) – Southern Connection

Existing peak hour traffic volumes were determined based on traffic counts conducted at the study intersection listed above, in April of 2023 by Burns Service, Inc. during a typical weekday AM (7:00 AM – 9:00 AM) and PM (4:00 PM – 6:00 PM) peak periods. Traffic volumes were balanced between study intersections, where appropriate.

### **3. Future Traffic Conditions**

Through coordination with the NCDOT and the WMPO, it was determined that an annual growth rate of 1.5% would be used to generate 2027 projected weekday AM and PM peak





hour traffic volumes. The following adjacent developments were identified to be included as an approved adjacent development in this study:

- Amberleigh Shores Phase 2
- Bailey Shoppes on Market
- Bayshore Commercial

After scoping, it was determined that only the Bailey Shoppes on Market development would generate trips along the study area. Additionally, based on coordination with the NCDOT and the WMPO, it was determined that the roadway improvements associated with the NCDOT State Transportation Improvement Program (STIP) project U-4902D should be considered in this study.

#### 4. Site Trip Generation

Average weekday daily, AM peak hour, and PM peak hour trips for the proposed development were estimated using methodology contained within the ITE Trip Generation Manual, 11.1<sup>th</sup> Edition. Table E-1 provides a summary of the trip generation potential for the site.

**Table E-1: Site Trip Generation**

Land Use (ITE Code)	Intensity	Daily Traffic (vpd)	Weekday AM Peak Hour Trips (vph)		Weekday PM Peak Hour Trips (vph)	
			Enter	Exit	Enter	Exit
Low-Rise Apartments (220)	253 DU	1,698	24	77	81	48

To estimate traffic conditions with the site fully built-out, the total site trips were added to the 2027 no-build traffic volumes to determine the 2027 build traffic volumes. The study analyzes traffic conditions during the weekday AM and PM peak hours for the following scenarios:

- 2023 Existing Traffic Conditions
- 2027 No-Build Traffic Conditions
- 2027 Build Traffic Conditions

#### 5. Capacity Analysis Summary

The analysis considered weekday AM and PM peak hour traffic for 2023 existing, 2027 no-build, and 2027 build conditions. Refer to Section 7 of the TIA for the capacity analysis summary performed at each study intersection.

## **6. Recommendations**

Based on the findings of this study, no specific geometric or traffic control improvements have been recommended as part of this development. Geometric and traffic control improvements are to be provided as via NCDOT STIP U-4902D. The improvements are summarized below and are illustrated in Figure E-1.

### **Improvements by STIP U-4902D**

#### **US-17 and SR 1345 (Alexander Road) – Northern Connection**

- Construct median along US-17.
- Restrict western leg (of Alexander Road) to eliminate left-turn egress movement.
- Restrict eastern leg (of Avery Drive) to eliminate left-turn ingress and egress movements.
- Provide northbound left-turn lane with at least 375 feet of full-width storage and appropriate decel and taper.
- Install two-phase traffic signal.

#### **US-17 and SR 1345 (Alexander Road) – Southern Connection**

- Construct median along US-17.
- Restrict western leg (of Alexander Road) to eliminate left-turn egress movement.
- Restrict eastern leg to eliminate left-turn ingress and egress movements.
- Provide northbound left-turn lane with at least 375 feet of full-width storage and appropriate decel and taper.

### **Recommended Laneage by Developer**

#### **SR 1345 (Alexander Road) and Site Access A**

- Construct Site Access A as a full movement driveway with at least one ingress and one egress lane.
- Provide stop-control for the egress of Site Access A.

#### **SR 1345 (Alexander Road) and Site Access B**

- Construct Site Access B as a full movement driveway with at least one ingress and one egress lane.
- Provide stop-control for the egress of Site Access B.

**LEGEND**

- Unsignalized Intersection
- ◫ Signalized Intersection
- ▶ Right-In / Right-Out Intersection
- ◀ Left-Over Intersection
- Existing Lane
- x' Storage (In Feet)
- ◫ Signal Installation by STIP U-4902D
- Improvements by STIP U-4902D
- New Movements by Developer



Site Access B

Site Access A

Alexander Road

Avery Drive

100

U-Haul/Chiropractor Driveway

17

	Abberly Wilmington Wilmington, NC	Recommended Lane Configurations	
		Scale: Not to Scale	Figure E-1

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- Appendix A: Scoping Documentation
- Appendix B: Traffic Counts
- Appendix C: Signal Plans
- Appendix D: Adjacent Development Information
- Appendix E: Future Roadway Improvements
- Appendix F: Capacity Calculations – US-17 and SR 1345 (Alexander Road) – Northern Connection
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# TRAFFIC IMPACT ANALYSIS

## **ABBERLY WILMINGTON** **Wilmington, North Carolina**

### **1. INTRODUCTION**

The contents of this report present the findings of the Traffic Impact Analysis (TIA) conducted for the proposed Abberly Wilmington residential development that is to be located along Alexander Road in Wilmington, North Carolina. The purpose of this study is to determine the potential impacts to the surrounding transportation system created by traffic generated by the proposed development, as well as recommend improvements to mitigate the impacts.

The proposed development, anticipated to be completed in 2027, is assumed to consist of 253 low-rise apartments.

The study analyzes traffic conditions during the weekday AM and PM peak hours for the following scenarios:

- 2023 Existing Traffic Conditions
- 2027 No-Build Traffic Conditions
- 2027 Build Traffic Conditions

#### **1.1. Site Location and Study Area**

The development is proposed to be located along Alexander Road in Wilmington, North Carolina. Refer to Figure 1 for the site location map.

The study area for the TIA was determined through coordination with the North Carolina Department of Transportation (NCDOT) and the Wilmington Metropolitan Planning Organization (WMPO) and consists of the following existing intersections:

- US-17 and SR 1345 (Alexander Road) – Northern Connection
- US-17 and SR 1345 (Alexander Road) – Southern Connection

#### **1.2. Proposed Land Use and Site Access**

The proposed development is assumed to consist of 253 low-rise apartments.





Site access is proposed via two full movement driveways along Alexander Road. An additional emergency access (for emergency vehicles only) is proposed near the rear of the development at Alexander Place. Refer to Figure 2 for a copy of the preliminary site plan. Refer to Appendix A for scoping documentation.

### 1.3. Adjacent Land Uses

The proposed development is located in an area consisting primarily of residential and commercial development.

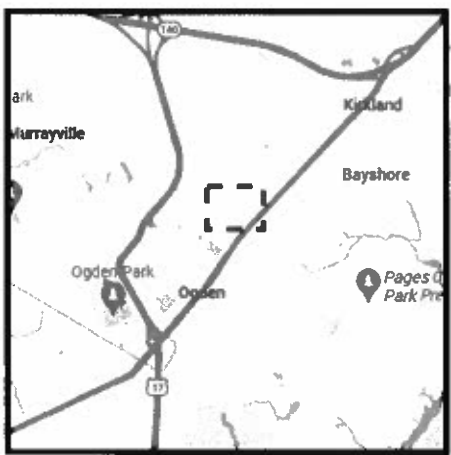
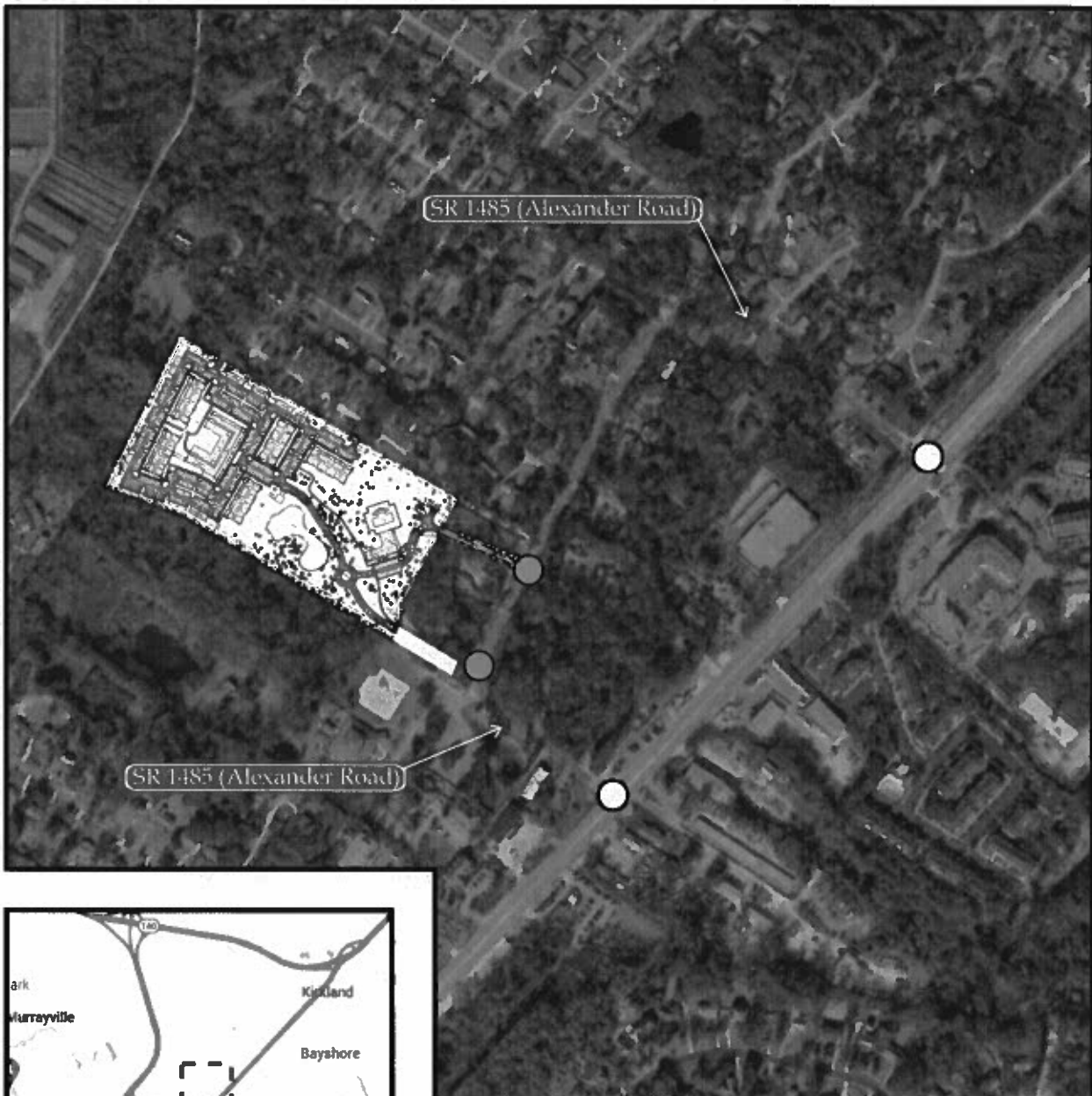
### 1.4. Existing Roadways

Existing lane configurations (number of traffic lanes on each intersection approach), speed limits, storage capacities, and other intersection and roadway information within the study area are shown in Figure 3. Table 1 provides a summary of this information, as well.

**Table 1: Existing Roadway Inventory**

Road Name	Route Number	Typical Cross Section	Speed Limit	AADT (vpd)
US-17 (Market Street)	US-17	4-lane divided (TWLTL)	45 mph	47,500
Alexander Road	SR 1345	2-lane undivided	25 mph	850
Avery Drive	N/A	2-lane undivided	Not Posted (25 mph assumed)	760*

\*ADT based on the traffic counts from 2023 and assuming the weekday PM peak hour volume is 10% of the average daily traffic.



**LEGEND**

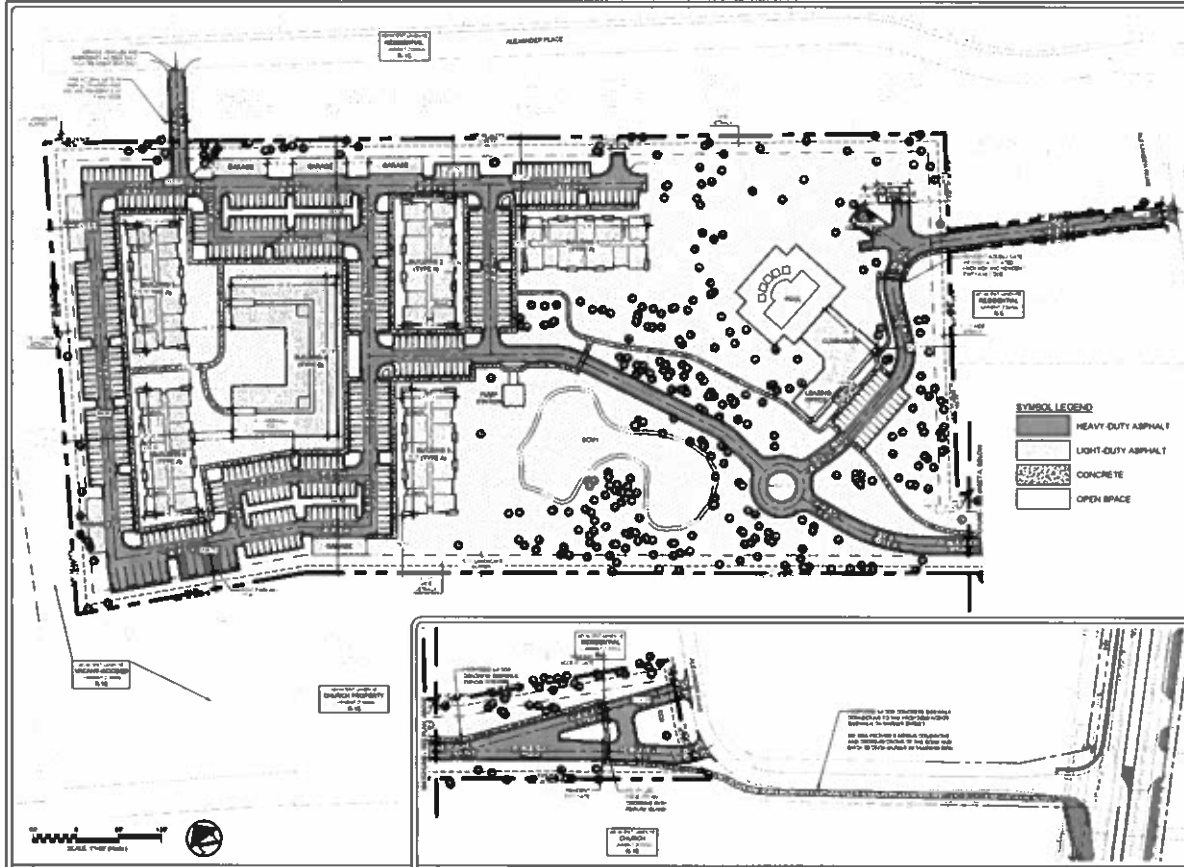
- Study Intersection
- Proposed Site Access
- ▭ Study Area



Abberly Wilmington  
Wilmington, NC

Site Location Map

Scale: Not to Scale    Figure 1



SITE DATA	
PROJECT NAME	ABBERLY AT WILMINGTON
CLIENT	ABBERLY AT WILMINGTON
DESIGNER	MFKIM & CREED
DATE	08/14/2019
SHEET NO.	1
TOTAL SHEETS	1
PROJECT LOCATION	1500 N. WILMINGTON BLVD., WILMINGTON, DE 19801
PROJECT TYPE	APARTMENTS
OWNER	ABBERLY AT WILMINGTON
DESIGNER	MFKIM & CREED
DATE	08/14/2019
SCALE	1" = 80'
<p>SYMBOL LEGEND</p> <ul style="list-style-type: none"> <li>HEAVY-DUTY ASPHALT</li> <li>LIGHT-DUTY ASPHALT</li> <li>CONCRETE</li> <li>OPEN SPACE</li> </ul>	

NOTE	
<p>1. All dimensions between end lines shall be given to face of curb or face of wall unless otherwise indicated. Dimensions shall be given to center of wall unless otherwise indicated. All dimensions shall be given to center of wall unless otherwise indicated.</p>	
<p>2. All dimensions shall be given to face of curb or face of wall unless otherwise indicated. Dimensions shall be given to center of wall unless otherwise indicated.</p>	
<p>3. All dimensions shall be given to face of curb or face of wall unless otherwise indicated. Dimensions shall be given to center of wall unless otherwise indicated.</p>	
<p>4. All dimensions shall be given to face of curb or face of wall unless otherwise indicated. Dimensions shall be given to center of wall unless otherwise indicated.</p>	

3 SITE PLAN - OVERALL SCALE 1" = 80'

FOR REVIEW AND COMMENT ONLY - NOT FOR CONSTRUCTION

2 INSET A - DRIVEWAY CONNECTION SCALE 1" = 80'


1 LEGEND AND NOTES

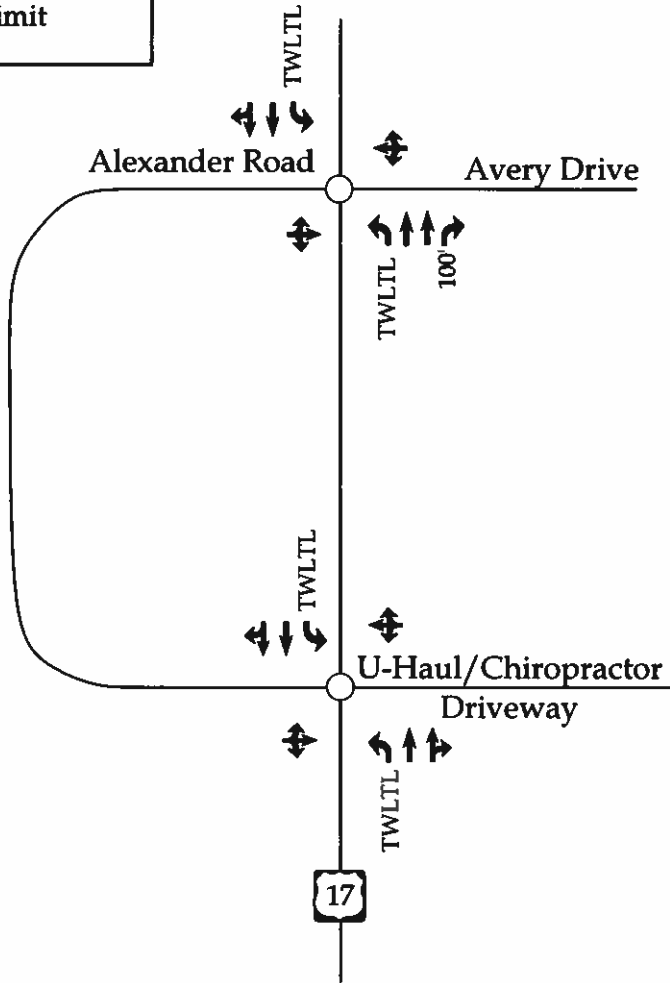
NO.	DATE	SCALE	CS 0
1	08/14/2019	1" = 80'	0

PRE-TRC SUBMITTAL - REV 4



**LEGEND**

- Unsignalized Intersection
- ➔ Existing Lane
- X' Storage (In Feet)
-  Posted Speed Limit



 <b>DRMP</b>	<b>Abberly Wilmington</b> <b>Wilmington, NC</b>	<b>2023 Existing Lane Configurations</b>	
		Scale: Not to Scale	Figure 3

## **2. 2023 EXISTING PEAK HOUR CONDITIONS**

### **2.1. 2023 Existing Peak Hour Traffic Volumes**

Existing peak hour traffic volumes were determined based on traffic counts conducted at the study intersections listed below, in April of 2023 by Burns Service, Inc. during a typical weekday AM (7:00 AM – 9:00 AM) and PM (4:00 PM – 6:00 PM) peak periods:

- US-17 and SR 1345 (Alexander Road) – Northern Connection
- US-17 and SR 1345 (Alexander Road) – Southern Connection

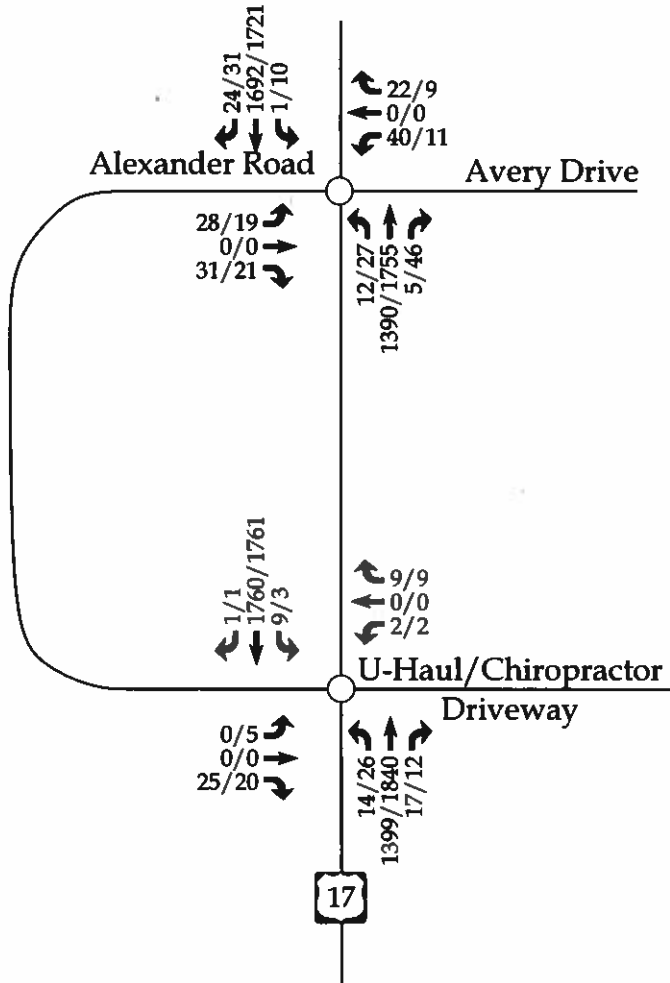
Refer to Figure 4 for 2023 existing weekday AM and PM peak hour traffic volumes. A copy of the count data is located in Appendix B of this report.

### **2.2. Analysis of Existing Peak Hour Traffic Conditions**

The 2023 existing weekday AM and PM peak hour traffic volumes were analyzed to determine the current levels of service at the study intersections under existing roadway conditions. Signal information was obtained from NCDOT and is included in Appendix C. The results of the analysis are presented in Section 7 of this report.

**LEGEND**

- Unsignalized Intersection
- X/Y → Weekday AM / PM Peak Hour Traffic



Note: Based on NCDOT Congestion Management guidelines, a volume of 4 vehicles per hour (vph) was analyzed for any movement with less than 4 vph.

	Abberly Wilmington Wilmington, NC	2023 Existing Peak Hour Traffic	
		Scale: Not to Scale	Figure 4

### 3. 2027 NO-BUILD PEAK HOUR CONDITIONS

In order to account for growth of traffic and subsequent traffic conditions at a future year, no-build traffic projections are needed. No-build traffic is the component of traffic due to the growth of the community and surrounding area that is anticipated to occur regardless of whether or not the proposed development is constructed. No-build traffic is comprised of existing traffic growth within the study area and additional traffic created as a result of adjacent approved developments.

#### 3.1. Ambient Traffic Growth

Through coordination with the NCDOT and the WMPO, it was determined that an annual growth rate of 1.5% would be used to generate 2027 projected weekday AM and PM peak hour traffic volumes. Refer to Figure 5 for 2027 projected peak hour traffic.

#### 3.2. Adjacent Development Traffic

Through coordination with the NCDOT and the WMPO, the following adjacent developments were identified to be included as an approved adjacent development in this study:

- Amberleigh Shores Phase 2
- Bailey Shoppes on Market
- Bayshore Commercial

Table 2, on the following page, provides a summary of the adjacent developments.

**Table 2: Adjacent Development Information**

Development Name	Location	Build-Out Year	Land Use / Intensity	TIA Performed
Bailey Shoppes on Market	Along Bump Along Road and, north of Alexander Road	2018*	4,800 s.f. office, 6,600 s.f. shopping center, 1,200 fast food, and 2,400 high-turnover restaurant	Davenport in February 2018

\*Development expected to be built out between existing conditions and build out of the proposed development.



It should be noted that the adjacent developments were approved, during scoping, by the NCDOT and the City. After scoping, it was determined that only the Bailey Shoppes on Market development would generate trips along the study area. Adjacent development trips are shown in Figure 6a. Adjacent development information can be found in Appendix D.

### **3.3. Future Roadway Improvements**

Based on coordination with the NCDOT and the WMPO, it was determined that the roadway improvements associated with the NCDOT State Transportation Improvement Program (STIP) project U-4902D should be considered in this study. STIP U-4902D is expected to install a median along US-17 that restricts both Alexander Road connections to US-17 to left-over intersections. A southbound u-turn bulb is to be constructed to the south of the southern connection and is expected to be signalized. The northern connection is also expected to be signalized. The existing traffic impacted by the restrictions was rerouted along US-17. Refer to Figure 6b for an illustration of the rerouted traffic as a result of the STIP project.

The roadway plans for STIP U-4902D can be found in Appendix E.

### **3.4. 2027 No-Build Peak Hour Traffic Volumes**

The 2027 no-build traffic volumes were determined by projecting the 2023 existing peak hour traffic to the year 2027, adding the adjacent development trips, and applying the trip diversion as a result of roadway improvements. Refer to Figure 7 for an illustration of the 2027 no-build peak hour traffic volumes at the study intersections.

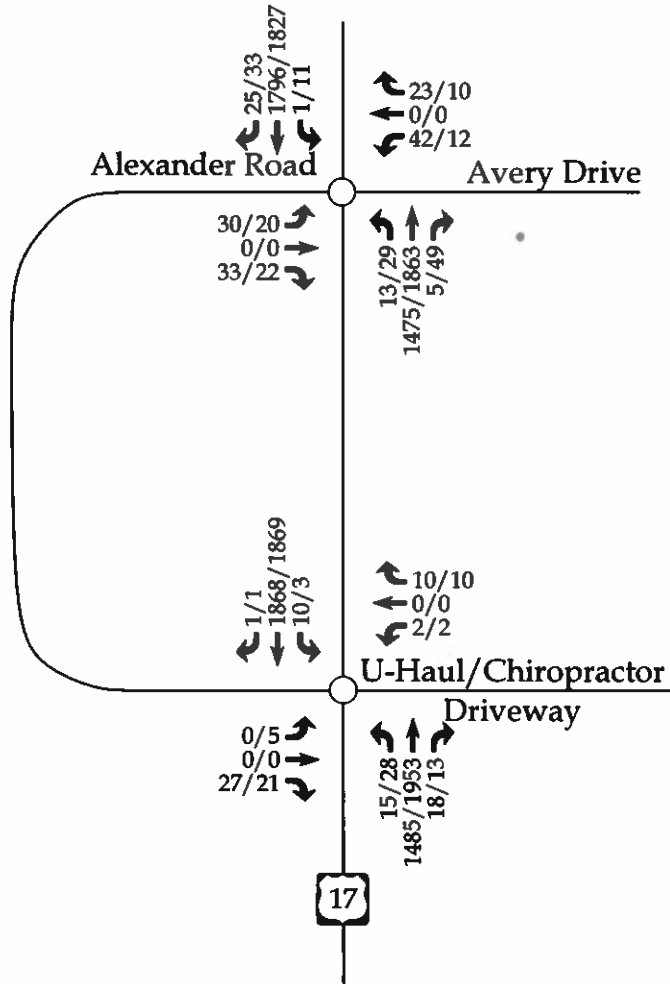
### **3.5. Analysis of No-Build Peak Hour Traffic Conditions**

The 2027 no-build AM and PM peak hour traffic volumes at the study intersections were analyzed with future geometric roadway conditions and traffic control. The analysis results are presented in Section 7 of this report.

**LEGEND**

○ Unsignalized Intersection

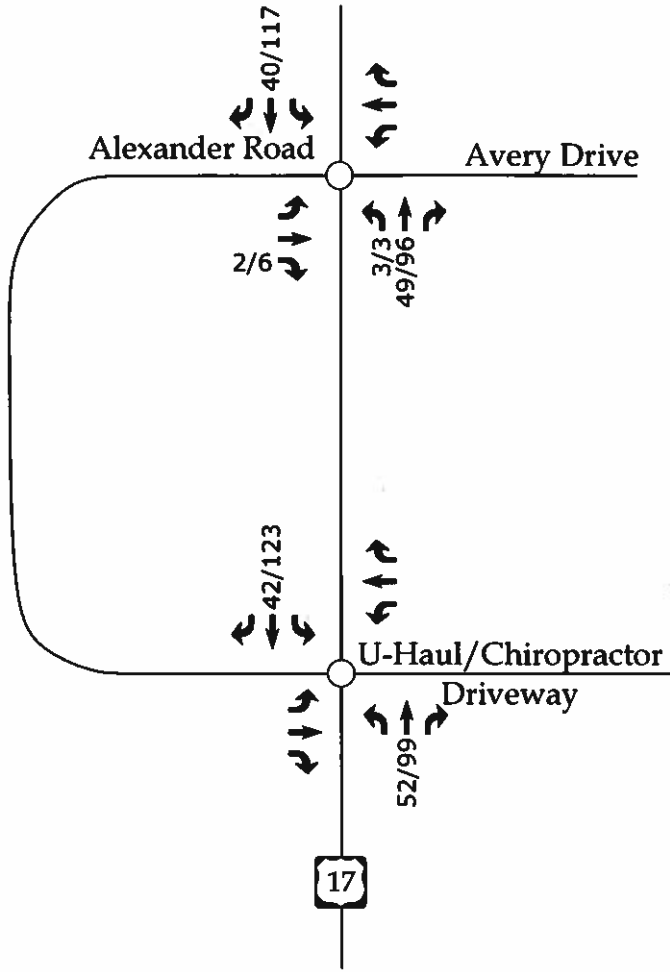
X/Y → Weekday AM / PM Peak Hour Traffic



	Abberly Wilmington Wilmington, NC	2027 Projected Peak Hour Traffic	
		Scale: Not to Scale	Figure 5

**LEGEND**

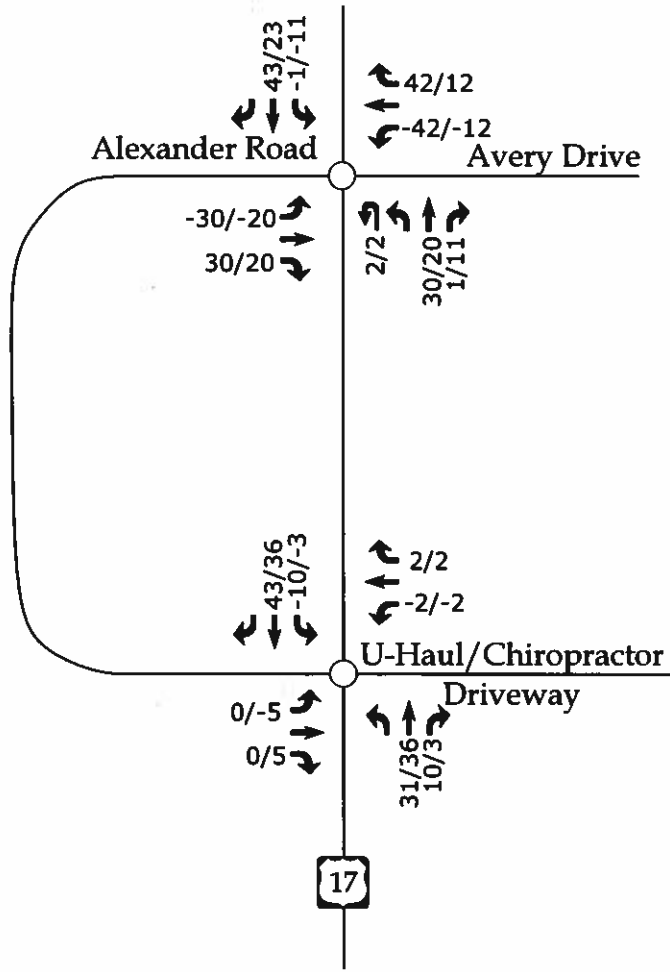
- Unsignalized Intersection
- X / Y → Weekday AM / PM Peak Hour Adjacent Development Trips



	<b>Abberly Wilmington</b> <b>Wilmington, NC</b>	<b>Peak Hour Adjacent Development Trips</b>	
		Scale: Not to Scale	Figure 6a

**LEGEND**

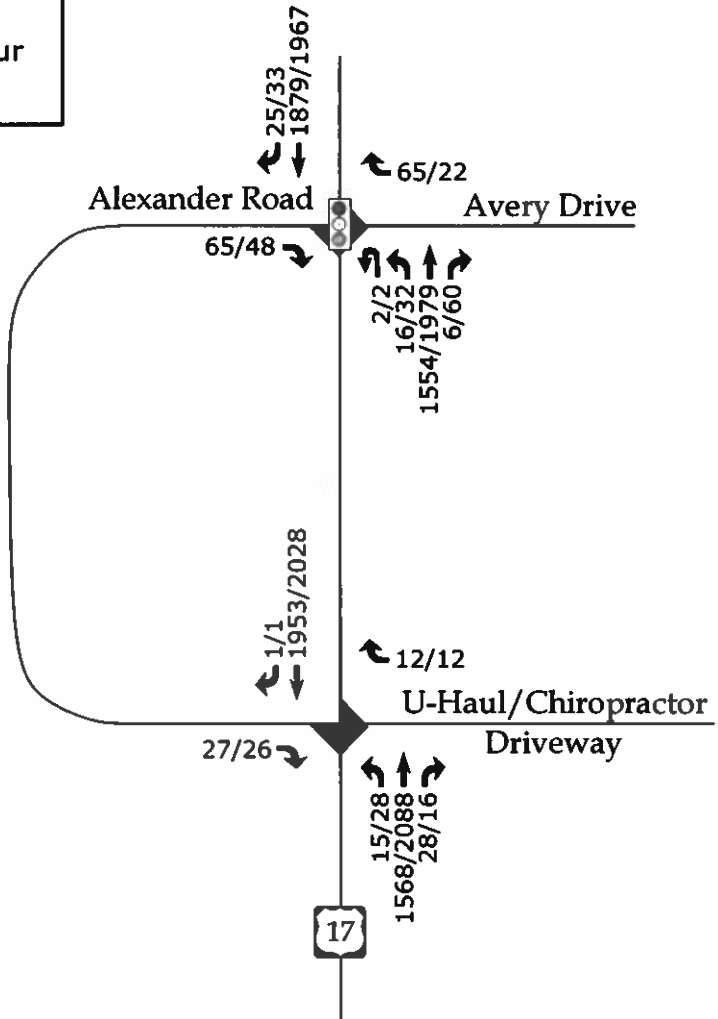
- Unsignalized Intersection
- X/Y → Weekday AM / PM Peak Hour Adjacent Development Trips



	<b>Abberly Wilmington</b> <b>Wilmington, NC</b>	<b>Trip Diversion</b>	
		Scale: Not to Scale	Figure 6b

**LEGEND**

- Unsignalized Intersection
- ◫ Signalized Intersection
- ▲ Right-In/Right-Out Intersection
- ◩ Left-Over Intersection
- X / Y → Weekday AM / PM Peak Hour Traffic



Note: Based on NCDOT Congestion Management guidelines, a volume of 4 vehicles per hour (vph) was analyzed for any movement with less than 4 vph.

	Abberly Wilmington Wilmington, NC	2027 No-Build Peak Hour Traffic	
		Scale: Not to Scale	Figure 7

## 4. SITE TRIP GENERATION AND DISTRIBUTION

### 4.1. Trip Generation

Average weekday daily, AM peak hour, and PM peak hour trips for the proposed development were estimated using methodology contained within the *ITE Trip Generation Manual*, 11.1 Edition. Table 3 provides a summary of the trip generation potential for the site.

**Table 3: Trip Generation Summary**

Land Use (ITE Code)	Intensity	Daily Traffic (vpd)	Weekday AM Peak Hour Trips (vph)		Weekday PM Peak Hour Trips (vph)	
			Enter	Exit	Enter	Exit
Low-Rise Apartments (220)	253 DU	1,698	24	77	81	48

It is estimated that the proposed development will generate approximately 1,698 total site trips on the roadway network during a typical 24-hour weekday period. Of the daily traffic volume, it is anticipated that 101 trips (24 entering and 77 exiting) will occur during the weekday AM peak hour and 129 trips (81 entering and 48 exiting) will occur during the weekday PM peak hour.

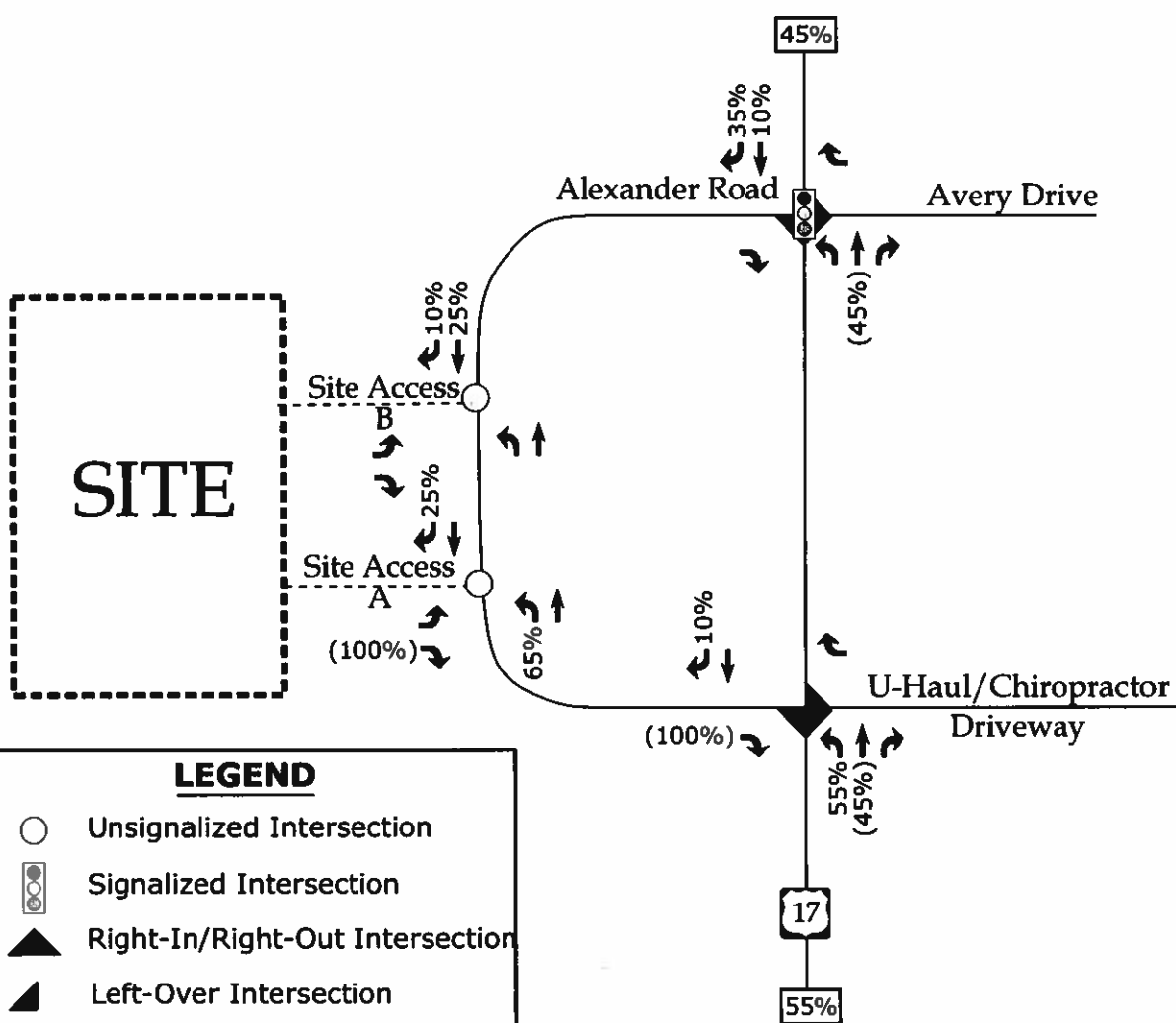
### 4.2. Site Trip Distribution and Assignment

Trip distribution percentages used in assigning site traffic for this development were estimated based on a combination of existing traffic patterns, population centers adjacent to the study area, and engineering judgment.

It is estimated that the site trips will be regionally distributed as follows:

- 55% to/from the south via US-17
- 45% to/from the north via US-17

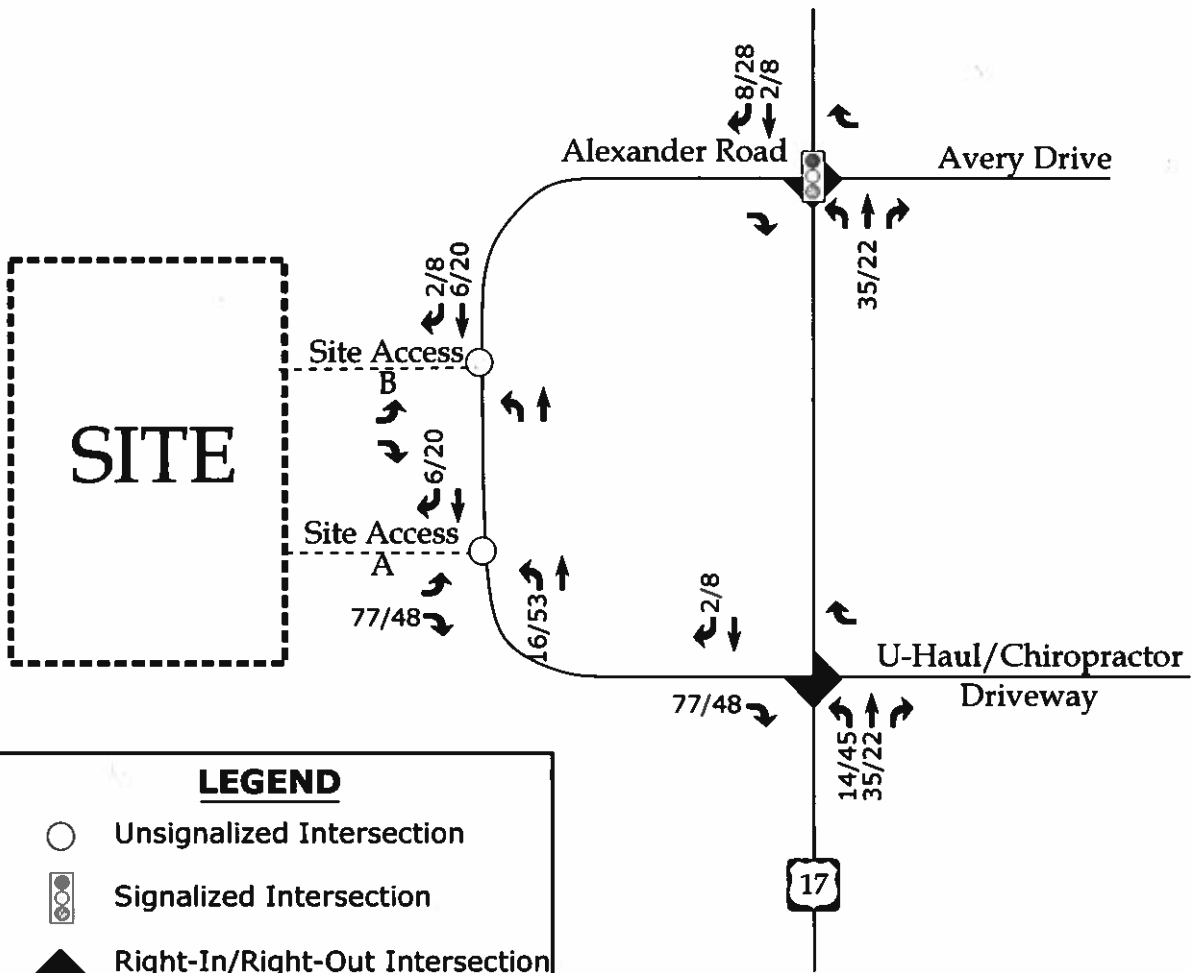
The site trip distribution is shown in Figure 8. Refer to Figure 9 for the site trip assignment.



**LEGEND**

- Unsignalized Intersection
- Ⓜ Signalized Intersection
- ▲ Right-In/Right-Out Intersection
- ▴ Left-Over Intersection
- x% → Entering Trip Distribution
- (y%) → Exiting Trip Distribution
- XX% Regional Trip Distribution

	Abberly Wilmington Wilmington, NC	Site Trip Distribution	
		Scale: Not to Scale	Figure 8



**LEGEND**

- Unsignalized Intersection
- ◻ Signalized Intersection
- ▲ Right-In/Right-Out Intersection
- ◄ Left-Over Intersection
- X / Y → Weekday AM / PM Peak Hour Site Trips

	Abberly Wilmington Wilmington, NC	Site Trip Assignment	
		Scale: Not to Scale	Figure 9



## **5. 2027 BUILD TRAFFIC CONDITIONS**

### **5.1. 2027 Build Peak Hour Traffic Volumes**

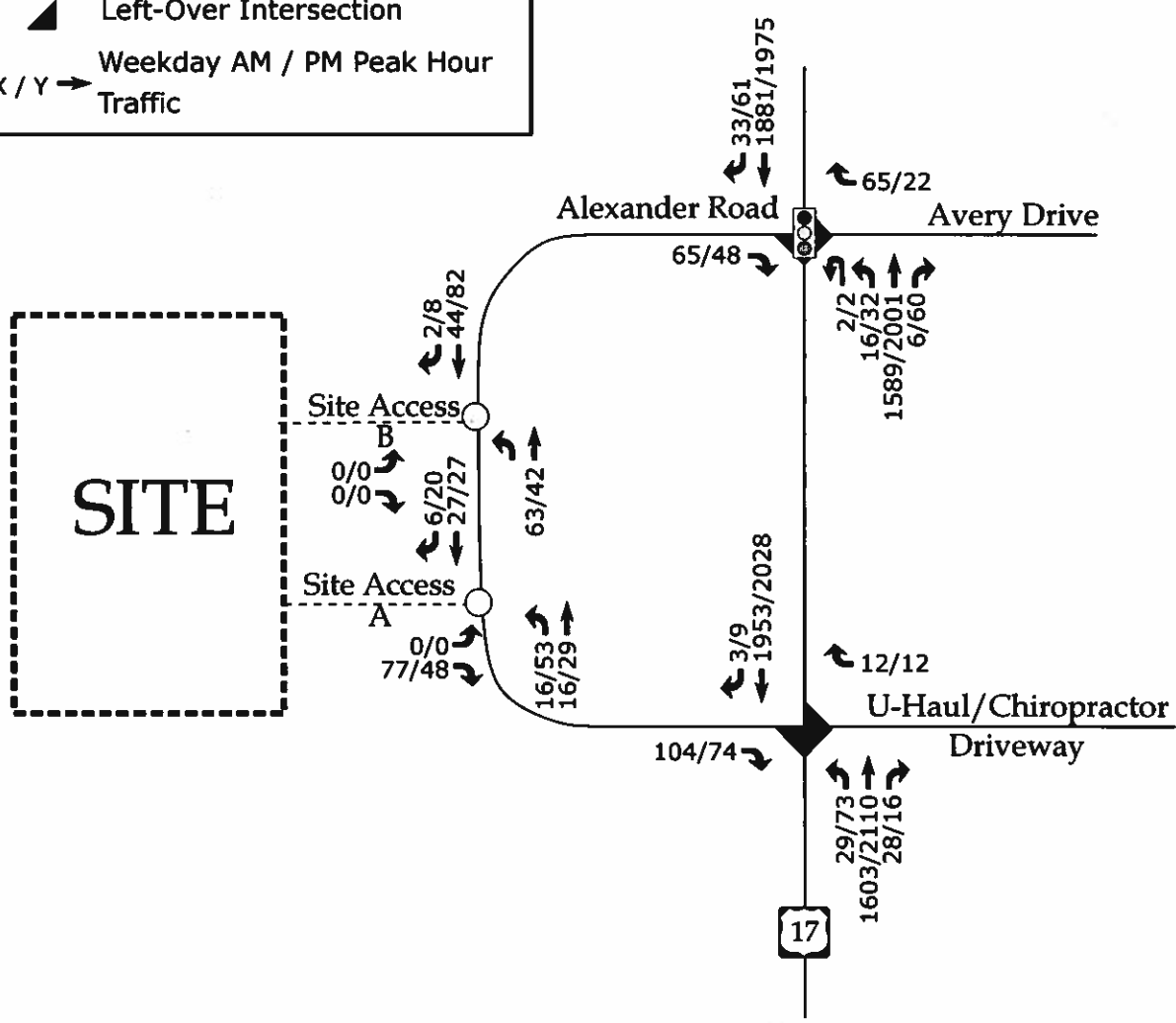
To estimate traffic conditions with the site fully built-out, the total site trips were added to the 2027 no-build traffic volumes to determine the 2027 build traffic volumes. Refer to Figure 10 for an illustration of the 2027 build peak hour traffic volumes with the proposed site fully developed.

### **5.2. Analysis of 2027 Build Peak Hour Traffic Conditions**

Study intersections were analyzed with the 2027 build traffic volumes using the same methodology previously discussed for no-build traffic conditions. The results of the capacity analysis for each intersection are presented in Section 7 of this report.

**LEGEND**

- Unsignalized Intersection
- ◫ Signalized Intersection
- ▲ Right-In/Right-Out Intersection
- ◄ Left-Over Intersection
- X / Y → Weekday AM / PM Peak Hour Traffic



Note: Based on NCDOT Congestion Management guidelines, a volume of 4 vehicles per hour (vph) was analyzed for any movement with less than 4 vph.

	<b>Abberly Wilmington</b> Wilmington, NC	<b>2027 Build</b> Peak Hour Traffic	
		Scale: Not to Scale	Figure 10

## 6. TRAFFIC ANALYSIS PROCEDURE

Study intersections were analyzed using the methodology outlined in the *Highway Capacity Manual (HCM)*, 6<sup>th</sup> Edition published by the Transportation Research Board. Capacity and level of service are the design criteria for this traffic study. A computer software package, Synchro (Version 11.1), was used to complete the analyses for the study area intersections. Please note that the unsignalized capacity analysis does not provide an overall level of service for an intersection; only delay for an approach with a conflicting movement.

The HCM defines capacity as “the maximum hourly rate at which persons or vehicles can reasonably be expected to traverse a point or uniform section of a lane or roadway during a given time period under prevailing roadway, traffic, and control conditions.” Level of service (LOS) is a term used to represent different driving conditions and is defined as a “qualitative measure describing operational conditions within a traffic stream, and their perception by motorists and/or passengers.” Level of service varies from Level “A” representing free flow, to Level “F” where breakdown conditions are evident. Refer to Table 4 for HCM levels of service and related average control delay per vehicle for both signalized and unsignalized intersections. Control delay as defined by the HCM includes “initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay”. An average control delay of 50 seconds at a signalized intersection results in LOS “D” operation at the intersection.

**Table 4: Highway Capacity Manual – Levels-of-Service and Delay**

Unsignalized Intersection		Signalized Intersection	
Level of Service	Average Control Delay Per Vehicle (Seconds)	Level of Service	Average Control Delay Per Vehicle (Seconds)
A	0-10	A	0-10
B	10-15	B	10-20
C	15-25	C	20-35
D	25-35	D	35-55
E	35-50	E	55-80
F	>50	F	>80

### 6.1. Adjustments to Analysis Guidelines

Capacity analysis at all study intersections was completed according to the NCDOT Congestion Management Guidelines.

## **7. CAPACITY ANALYSIS**

The following study intersections were analyzed under 2023 existing, 2027 no-build, and 2027 build traffic conditions:

- US-17 and SR 1345 (Alexander Road) – Northern Connection
- US-17 and SR 1345 (Alexander Road) – Southern Connection

All proposed site driveways were analyzed under 2027 build traffic conditions. Refer to Tables 5-8 for a summary of capacity analysis results. Refer to Appendices F-J for the Synchro capacity analysis reports and SimTraffic queueing reports. Refer to Appendix K for turn lane warrants at site driveways.

## 7.1. US-17 and Alexander Road – Northern Connection

Refer to the table below for a summary of the capacity analysis of the subject intersection during the analysis scenarios.

**Table 5: Analysis Summary of US-17 and Alexander Road – Northern Connection**

ANALYSIS SCENARIO	N O D E	LANE GROUP	Existing Storage (ft)	Weekday AM Peak Hour				Overall LOS (sec)	Weekday PM Peak Hour				Overall LOS (sec)
				Queue 95 <sup>th</sup> /Max (ft)	Lane LOS	Delay (sec)	Approach LOS (sec)		Queue 95 <sup>th</sup> /Max (ft)	Lane LOS	Delay (sec)	Approach LOS (sec)	
2023 Existing Conditions	1	EBLTR	--	258/707	F	4393	F (4393) <sup>2</sup>	N/A	0/489	--	--**	--	N/A
		WBLTR	--	268/1038	F	4580	F (4580) <sup>2</sup>		0/251	--	--**	--	
		NBL	125	3/37	C	17			8/46	C	19		
		NBT (2)	--	--	--	--	C (17) <sup>1</sup>		--	--	--	C (19) <sup>1</sup>	
		NBR	100	--	--	--			--	--	--		
		SBL	25	0/32	B	14			3/34	C	18		
		SBT	--	--	--	--	B (14) <sup>1</sup>	--	--	--	C (18) <sup>1</sup>		
		SBTR	--	--	--	--		0/4	--	--			
2027 No-Build Conditions	101	EBR	--	93/133	E	62	E (62)	A (8)	74/130	E	61	E (61)	A (8)
		NBUL*	375	38/56	D	47	D (47)		61/86	D	52	D (52)	
		SBT	--	423/194	A	6	A (6)		431/204	AA	6	A (6)	
			SBTR	--	423/177	A	6	A (6)	431/191	AA	6	A (6)	
	102	WBR	--	23/79	C	21	C (21) <sup>2</sup>	N/A	10/45	C	25	C (25) <sup>2</sup>	N/A
		NBT (2)	--	--	--	--	--		--	--	--	--	
		NBR	200	--	--	--		--	--	--	--		
2027 Build Conditions	101	EBR	--	94/122	E	62	E (62)	A (8)	74/117	E	60	E (60)	A (8)
		NBUL*	375	38/52	D	47	D (47)		61/89	D	52	D (52)	
		SBT	--	431/192	A	6	A (6)		458/192	A	6	A (6)	
			SBTR	--	431/199	A	6	A (6)	458/169	A	6	A (6)	
	102	WBR	--	25/88	C	22	C (22) <sup>2</sup>	N/A	10/54	D	25	D (25) <sup>2</sup>	N/A
		NBT (2)	--	--	--	--	--		--	--	--	--	
		NBR	200	--	--	--		--	--	--	--		

Improvements by STIP are underlined.

\*Synchro analyzed the NB U-Turn/Left as WB Left/Through movements due to the nature of the lane configurations and Synchro limitations.

\*\*Delay not reported due to high delays and Synchro limitations.

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.

Capacity analysis of existing traffic conditions indicates the minor street approaches operate at LOS F during the AM and PM peak hour. It should be noted that delays are not reported for the minor street approaches during the PM peak hour due to the significance of the delays.

Under no-build conditions, the western side of the intersection is expected to be converted to a signalized reduced conflict intersection (RCI). The eastern side of the intersection is to be restricted to right turn movements only. These modifications are to be made by

NCDOT STIP U-4902. Under future conditions, the signalized intersection is expected to operate at LOS A during the AM and PM peak hour. The westbound minor street is expected to operate at LOS D or better during the AM and PM peak hour. No significant queuing is expected at the intersection under future conditions.

No improvements are recommended by the developer.

## 7.2. US-17 and Alexander Road – Southern Connection

Refer to the table below for a summary of the capacity analysis of the subject intersection during the analysis scenarios.

**Table 6: Analysis Summary of US-17 and Alexander Road– Southern Connection**

ANALYSIS SCENARIO	LANE GROUP	Existing Storage (ft)	Weekday AM Peak Hour				Overall LOS (sec)	Weekday PM Peak Hour				Overall LOS (sec)
			Queue 95 <sup>th</sup> /Max (ft)	Lane LOS	Delay (sec)	Approach LOS (sec)		Queue 95 <sup>th</sup> /Max (ft)	Lane LOS	Delay (sec)	Approach LOS (sec)	
2023 Existing Conditions	EBLTR	--	140/239	F	1733	F (1733) <sup>2</sup>	N/A	0/254	--	--	--	N/A
	WBLTR	--	90/100	F	2129	F (2129) <sup>2</sup>		0/184	--	--	--	
	NBL	25	5/26	C	18	C (18) <sup>1</sup>		8/44	C	19	C (19) <sup>1</sup>	
	NBT	--	--	--	--			--	--	--		
	NBTR	--	--	--	--	--		--	--	--	--	
	SBL	25	3/24	B	14	B (14) <sup>1</sup>		3/22	C	19	C (19) <sup>1</sup>	
SBT	--	--	--	--	--		--	--	--			
SBTR	--	--	--	--	--		--	--	--			
2027 No-Build Conditions	<u>EBR</u>	--	13/64	C	25	C (25) <sup>2</sup>	N/A	13/66	D	26	D (26) <sup>2</sup>	N/A
	<u>WBR</u>	--	3/48	C	18	C (18) <sup>2</sup>		5/51	D	26	D (26) <sup>2</sup>	
	NBL	<u>325</u>	5/45	C	21	C (21) <sup>1</sup>		13/62	C	24	C (24) <sup>1</sup>	
	NBT	--	--	--	--			--	--	--		
	NBTR	--	--	--	--	--		--	--	--	--	
	SBT	--	--	--	--	--		--	--	--	--	
SBTR	--	0/8	--	--	--	--	--	--	--			
2027 Build Conditions	EBR	--	73/136	E	41	E (41) <sup>2</sup>	N/A	48/103	E	36	E (36) <sup>2</sup>	N/A
	WBR	--	3/35	C	19	C (19) <sup>2</sup>		5/56	D	26	D (26) <sup>2</sup>	
	NBL	325	13/46	C	22	C (22) <sup>1</sup>		40/106	D	30	D (30) <sup>1</sup>	
	NBT	--	--	--	--			--	--	--		
	NBTR	--	--	--	--	--		--	--	--	--	
	SBT	--	--	--	--	--		--	--	--	--	
SBTR	--	--	--	--	--	--	--	--	--			

Improvements by STIP are underlined.

\*Delay not reported due to high delays and Synchro limitations.

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.

Capacity analysis of existing traffic conditions indicates the minor-street approaches currently operate at LOS F while the major street left turn movements operate at LOS C or better during the AM and PM peak hour. Under no-build conditions, the western side of the intersection is expected to be converted to an unsignalized RCI. The eastern side is to be restricted to right turn movements only. These modifications are to be made by NCDOT STIP U-4902. Under future traffic conditions, the minor-street approaches are expected to operate at LOS E or better during the weekday AM and PM peak hours with maximum queues of less than 140 feet (between 5-6 cars).

No improvements are recommended by the developer.



### 7.3. Alexander Road and Site Access A

Refer to the table below for a summary of the capacity analysis of the subject intersection during the analysis scenarios.

**Table 7: Analysis Summary of SR 1345 (Alexander Road) and Site Access A**

ANALYSIS SCENARIO	LANE GROUP	Existing Storage (ft)	Weekday AM Peak Hour				Overall LOS (sec)	Weekday PM Peak Hour				Overall LOS (sec)
			Queue 95 <sup>th</sup> / Max (ft)	Lane LOS	Delay (sec)	Approach LOS (sec)		Queue 95 <sup>th</sup> / Max (ft)	Lane LOS	Delay (sec)	Approach LOS (sec)	
2027 Build Conditions	<b>EBLR</b>	--	<b>8/63</b>	<b>A</b>	<b>9</b>	<b>A (9)<sup>2</sup></b>	N/A	<b>5/54</b>	<b>A</b>	<b>9</b>	<b>A (9)<sup>2</sup></b>	N/A
	<b>NBLT</b>	--	<b>0/12</b>	<b>A</b>	<b>7</b>	<b>A (7)<sup>1</sup></b>		<b>3/25</b>	<b>A</b>	<b>7</b>	<b>A (7)<sup>1</sup></b>	
	<b>SBTR</b>	--	--	--	--	--		--	--	--	--	

Lane configuration modifications made by developer shown in **bold**.

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.

Capacity analysis of build traffic conditions indicates the minor-street movement and major-street left-turn movement are expected to operate at LOS A during the AM and PM peak hour. No significant queuing is expected at the intersection.

Left-turn lanes were considered based on the NCDOT *Policy on Street and Driveway Access to North Carolina Highways* and are not recommended.

No improvements are recommended by the developer.

## 7.4. SR 1345 (Alexander Road) and Site Access B

Refer to the table below for a summary of the capacity analysis of the subject intersection during the analysis scenarios.

**Table 8: Analysis Summary of SR 1345 (Alexander Road) and Site Access B**

ANALYSIS SCENARIO	LANE GROUP	Existing Storage (ft)	Weekday AM Peak Hour				Overall LOS (sec)	Weekday PM Peak Hour				Overall LOS (sec)
			Queue 95 <sup>th</sup> Max (ft)	Lane LOS	Delay (sec)	Approach LOS (sec)		Queue 95 <sup>th</sup> Max (ft)	Lane LOS	Delay (sec)	Approach LOS (sec)	
2027 Build Conditions	<b>EBLR</b>	--	<b>0/31</b>	<b>A</b>	<b>9</b>	<b>A (9)<sup>2</sup></b>	N/A	<b>0/31</b>	<b>A</b>	<b>9</b>	<b>A (9)<sup>2</sup></b>	N/A
	<b>NBLT</b>	--	<b>0/6</b>	<b>A</b>	<b>7</b>	<b>A (7)<sup>1</sup></b>		<b>0/0</b>	<b>A</b>	<b>7</b>	<b>A (7)<sup>1</sup></b>	
	<b>SBTR</b>	--	--	--	--	--		--	--	--	--	

Improvements to lane configurations are shown in bold.

1. Level of service for major-street left-turn movement.
2. Level of service for minor-street approach.

Capacity analysis of build traffic conditions indicates the minor-street movement and major-street left-turn movement are expected to operate at LOS A during the AM and PM peak hour. No significant queuing is expected at the intersection.

Left-turn lanes were considered based on the NCDOT *Policy on Street and Driveway Access to North Carolina Highways* and are not recommended.

No improvements are recommended by the developer.

## **8. CONCLUSIONS**

This Traffic Impact Analysis was conducted to determine the potential traffic impacts of the Abberly Wilmington residential development to be located along Alexander Road in Wilmington, North Carolina. The proposed development, anticipated to be completed in 2027, is assumed to consist of 253 low-rise apartments. Site access to the parcel is proposed via two full movement driveways along Alexander Road.

The study analyzes traffic conditions during the weekday AM and PM peak hours for the following scenarios:

- 2023 Existing Traffic Conditions
- 2027 No-Build Traffic Conditions
- 2027 Build Traffic Conditions

### **Trip Generation**

It is anticipated the development will generate 101 trips (24 entering and 77 exiting) during the weekday AM peak hour and 129 trips (81 entering and 48 exiting) during the weekday PM peak hour.

### **Adjustments to Analysis Guidelines**

Capacity analysis at all study intersections was completed according to NCDOT Congestion Management Guidelines. Refer to section 6.1 of this report for a detailed description of any adjustments to these guidelines made throughout the analysis.

## **9. RECOMMENDATIONS**

Based on the findings of this study, no specific geometric or traffic control improvements have been recommended as part of this development. Geometric and traffic control improvements are to be provided as via NCDOT STIP U-4902D. The improvements are summarized below and are illustrated in Figure 11.

### **Improvements by STIP U-4902D**

#### **US-17 and SR 1345 (Alexander Road) – Northern Connection**

- Construct median along US-17.
- Restrict western leg (of Alexander Road) to eliminate left-turn egress movement.
- Restrict eastern leg (of Avery Drive) to eliminate left-turn ingress and egress movements.
- Provide northbound left-turn lane with at least 375 feet of full-width storage and appropriate decel and taper.
- Install two-phase traffic signal.

#### **US-17 and SR 1345 (Alexander Road) – Southern Connection**

- Construct median along US-17.
- Restrict western leg (of Alexander Road) to eliminate left-turn egress movement.
- Restrict eastern leg to eliminate left-turn ingress and egress movements.
- Provide northbound left-turn lane with at least 375 feet of full-width storage and appropriate decel and taper.

### **Recommended Laneage by Developer**

#### **SR 1345 (Alexander Road) and Site Access A**

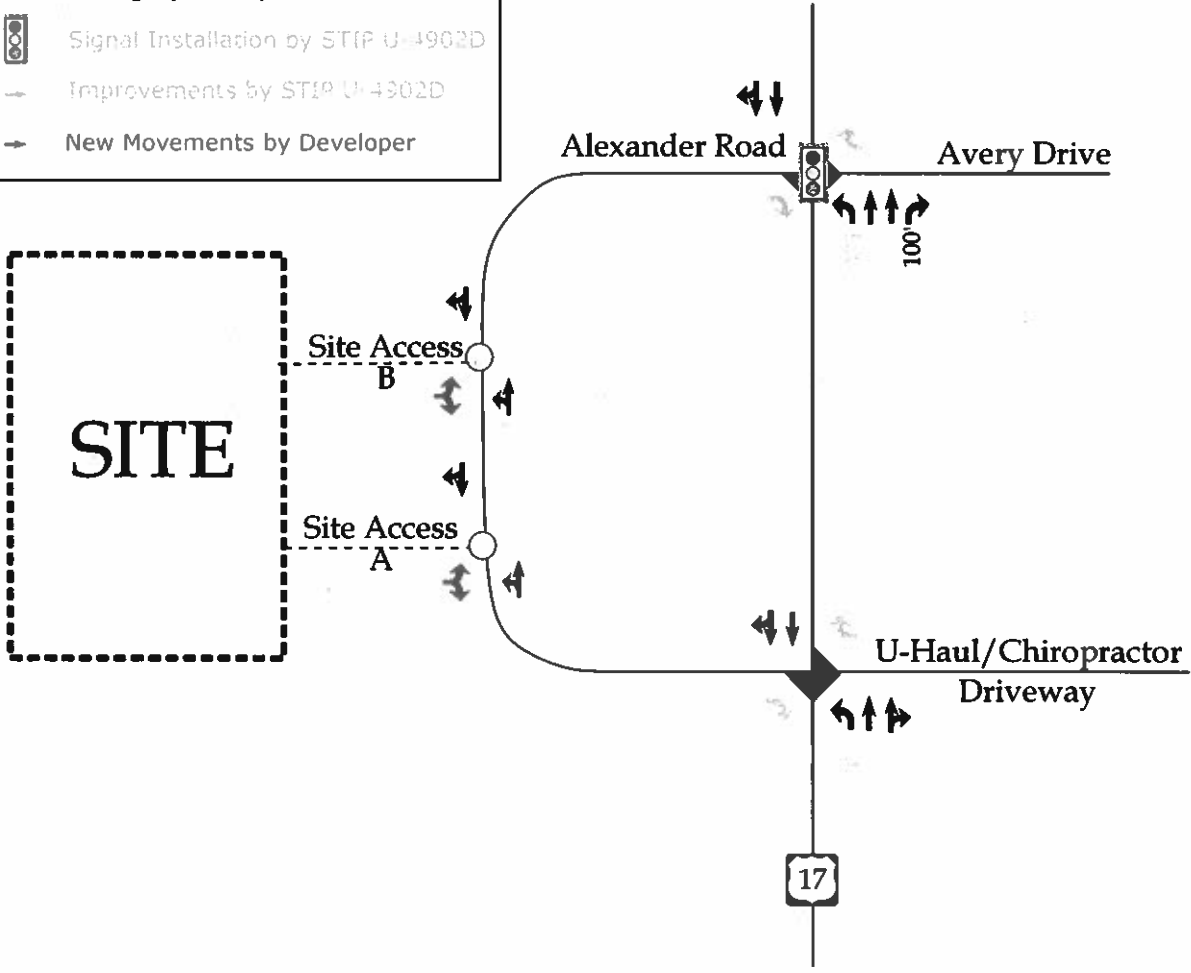
- Construct Site Access A as a full movement driveway with at least one ingress and one egress lane.
- Provide stop-control for the egress of Site Access A.

### SR 1345 (Alexander Road) and Site Access B

- Construct Site Access B as a full movement driveway with at least one ingress and one egress lane.
- Provide stop-control for the egress of Site Access B.

**LEGEND**

- Unsignalized Intersection
- ◫ Signalized Intersection
- ▶ Right-In / Right-Out Intersection
- ▲ Left-Over Intersection
- Existing Lane
- x' Storage (In Feet)
- ◫ Signal Installation by STIP U-4902D
- Improvements by STIP U-4902D
- New Movements by Developer



	Abberly Wilmington Wilmington, NC	Recommended Lane Configurations	
	Scale: Not to Scale		Figure 11